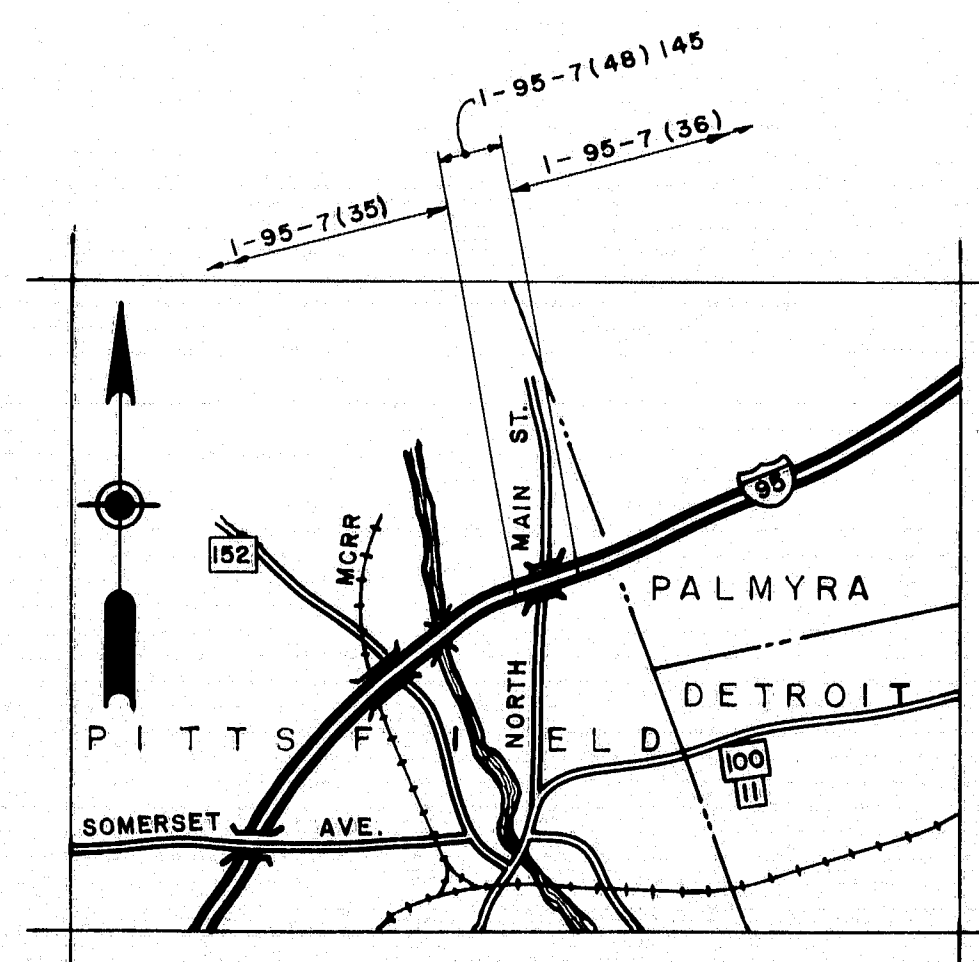


STATE OF MAINE
STATE HIGHWAY COMMISSION



INTERSTATE 95
OVER
NORTH MAIN STREET
IN THE TOWN OF
PITTSFIELD
SOMERSET COUNTY

FEDERAL AID PROJECT NO. I-95-7(48)145
LENGTH OF PROJECT 0.052 MILES



LOCATION MAP
APPROX. SCALE - 1" = 1 MILE

INDEX OF SHEETS

1	TITLE SHEET
2	GENERAL PLAN (ESTIMATE OF QUANTITIES)
3-4	FOUNDATION SURVEY
5	PROFILES - ROADWAY WORK
6	SOUTHBOUND CROSS SECTIONS
7	NORTHBOUND CROSS SECTIONS
8-9	CROSS SECTIONS - NORTH MAIN STREET
10	ABUTMENT NO. 1 - N.B.
11	ABUTMENT NO. 2 - N.B.
12	ABUTMENT NO. 1 - S.B.
13	ABUTMENT NO. 2 - S.B.
14	PIERS - NORTHBOUND & SOUTHBOUND
15	STRUCTURAL STEEL
16	BLOCKING SCHEDULE (ESTIMATE OF BRIDGE QUANTITIES)
17	SUPERSTRUCTURE
18	SLOPE PAVING
19	REINFORCING STEEL, GRANITE BRIDGE CURB, APPROACH SLAB DETAIL STANDARD DETAILS, BD 101-62, BD 102-62, BD 103-62

TRAFFIC

NORTH MAIN STREET	INTERSTATE 95
510	A.D.T. 1962 5830
715	A.D.T. 1982 7920
86	D.H.V. 950
11 %	T 11 %
60 %	D 60 %
45 MPH	V 60 MPH

APPROVED
MAINE STATE HIGHWAY COMMISSION

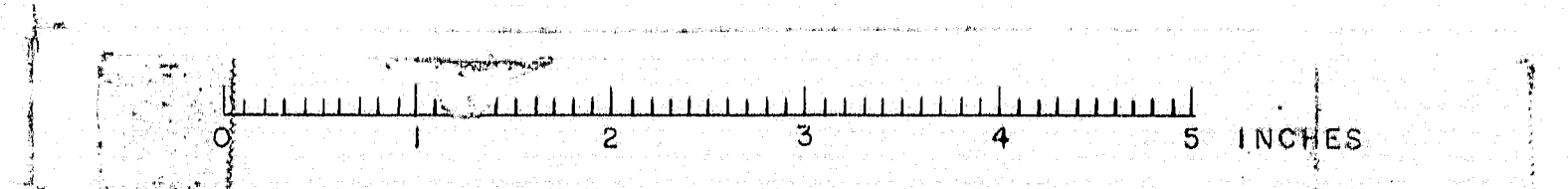
David W. Thomas
CHAIRMAN
E. J. ...
...
CHIEF ENGINEER

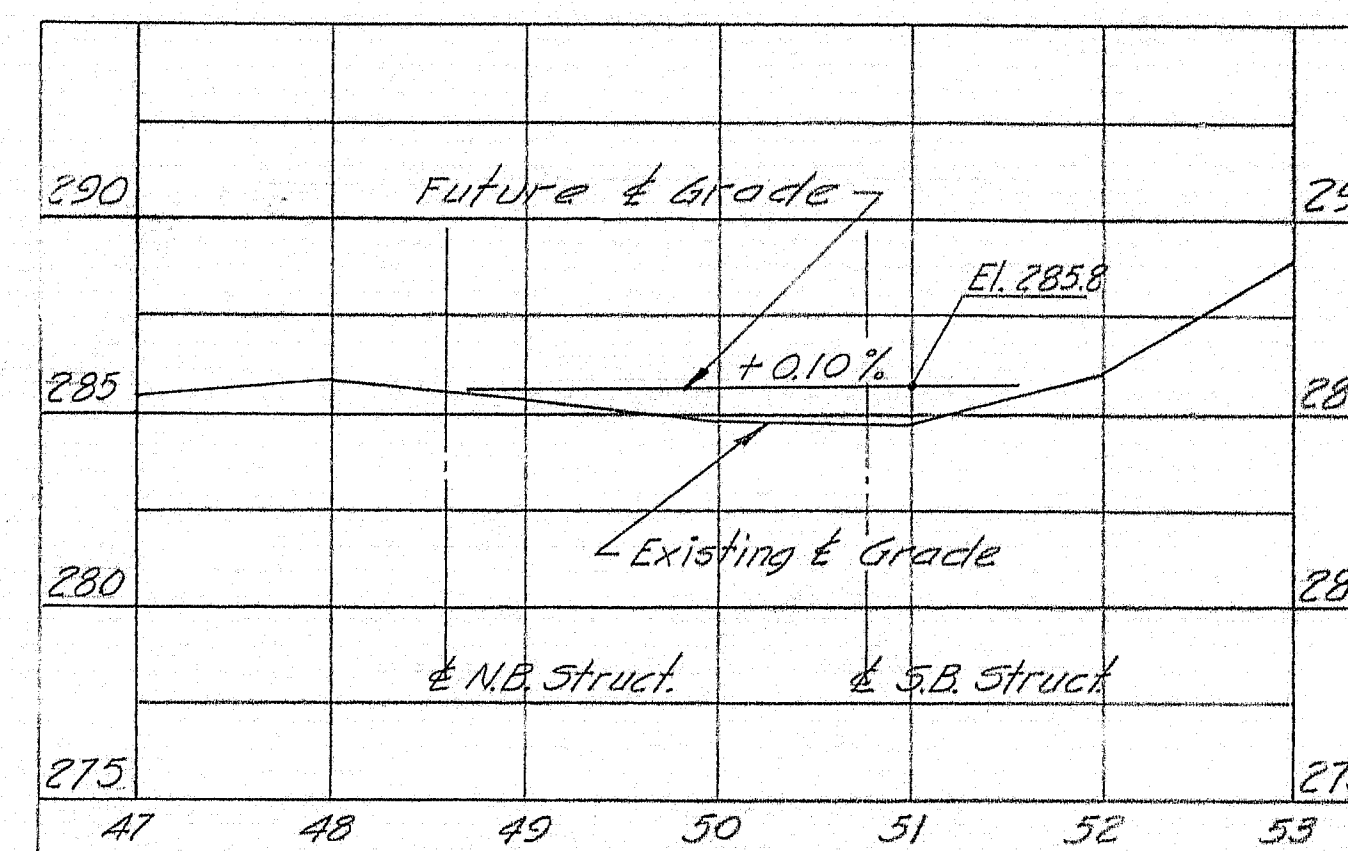
4-10-63
DATE

DEPARTMENT OF COMMERCE
BUREAU OF PUBLIC ROADS
REGION 1

APPROVED

DIVISION ENGINEER DATE



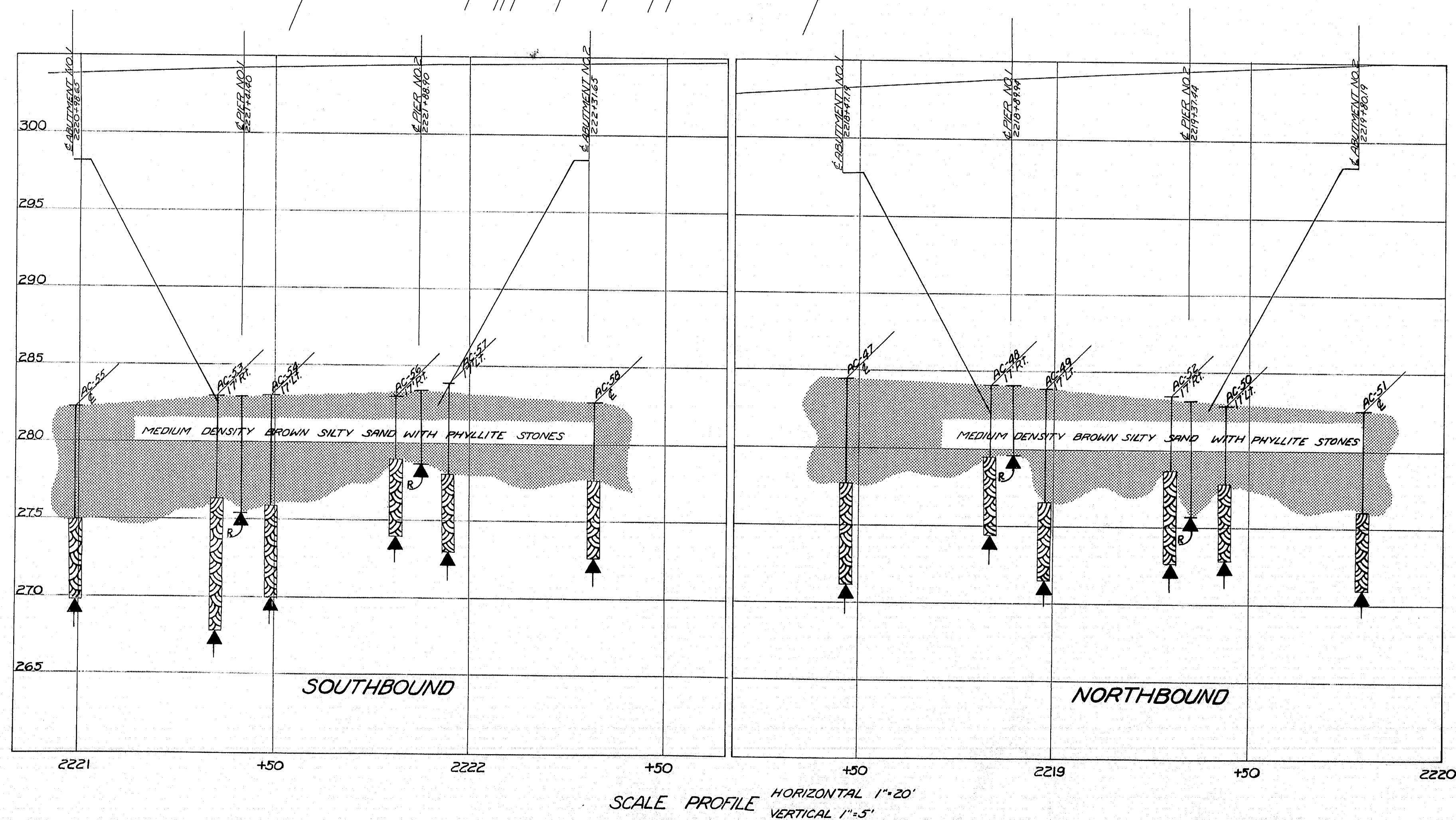
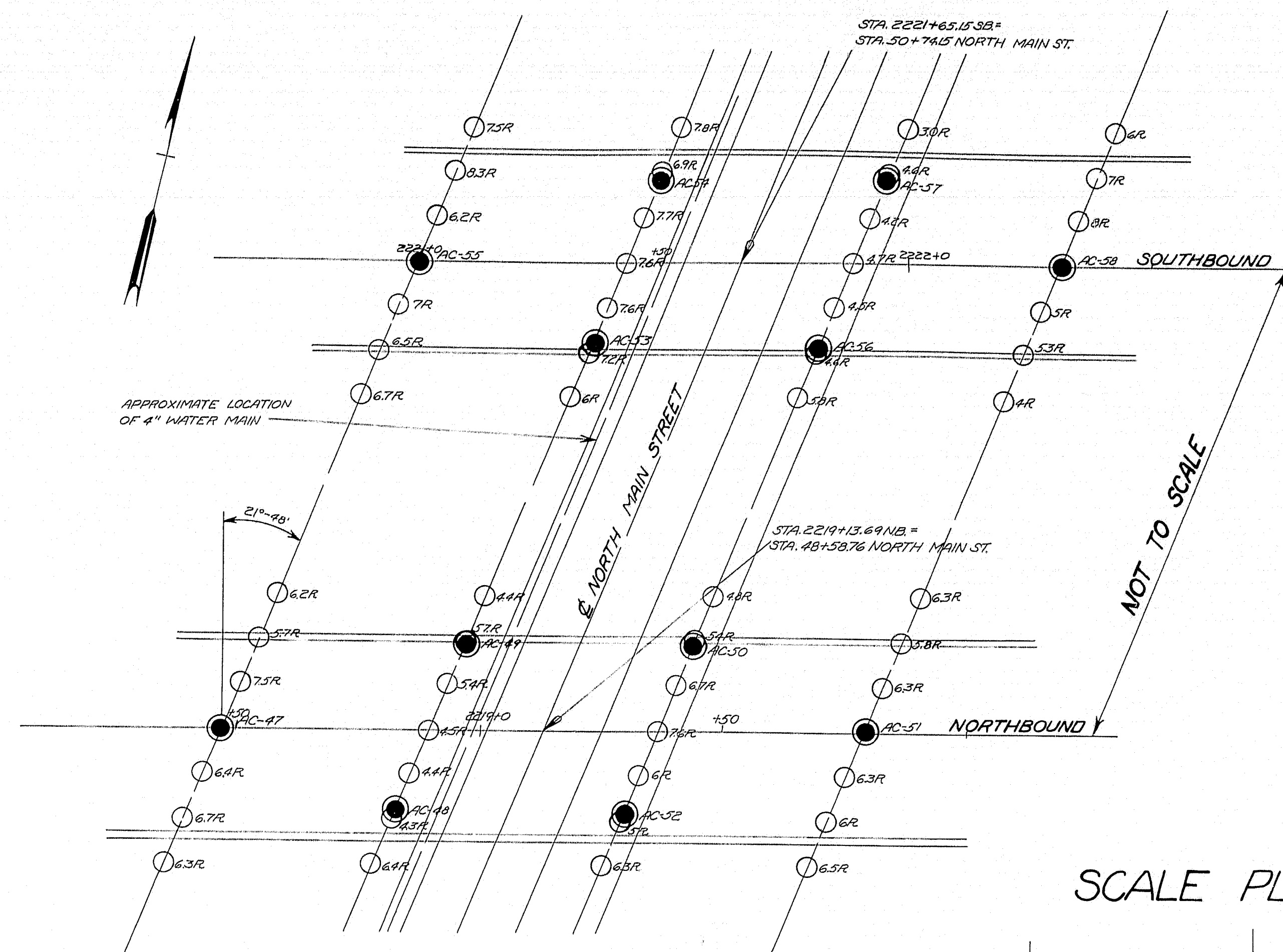


All concrete _____ Class "A"

For Bridge Quantities, see Sheet #16.

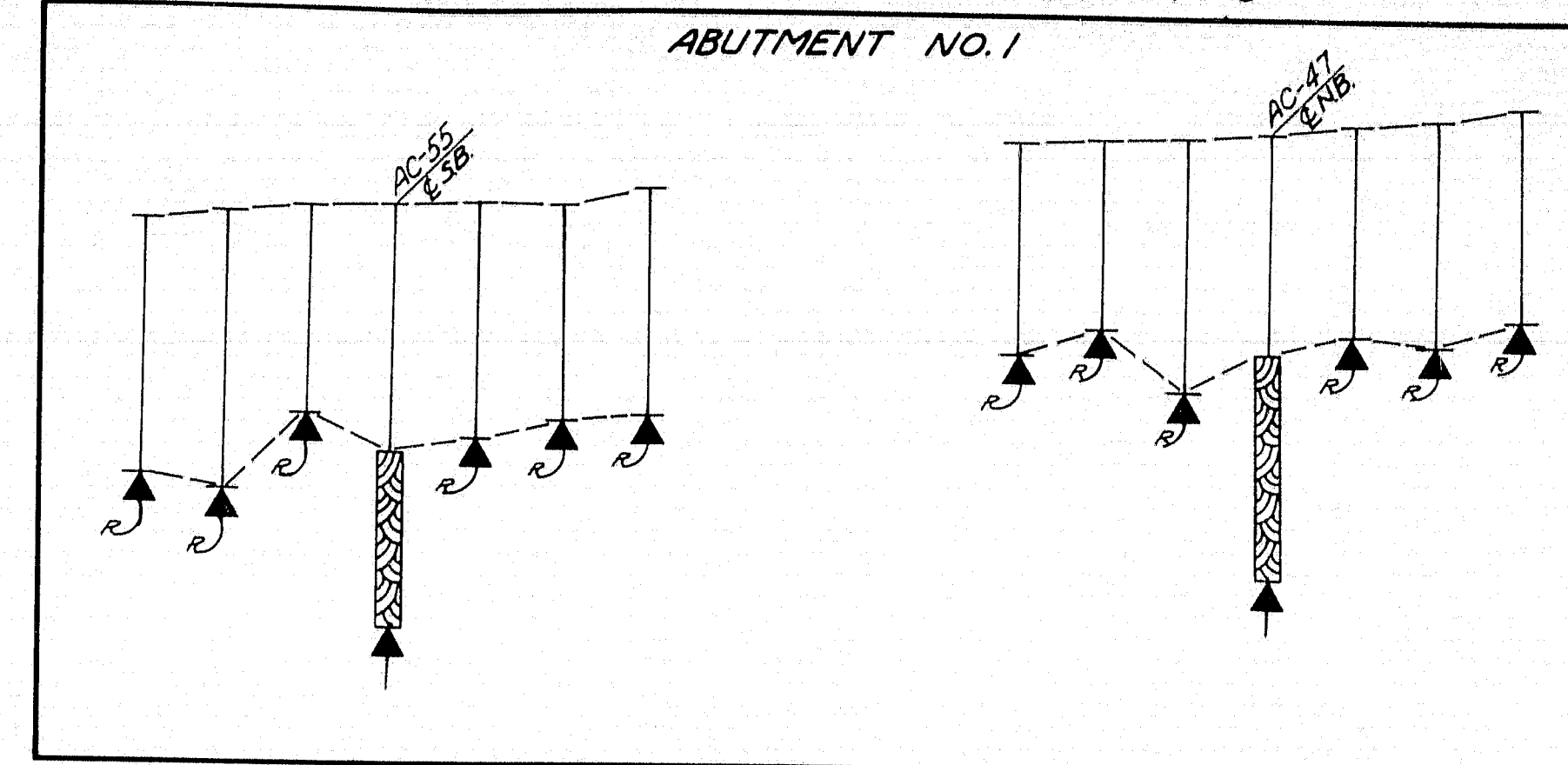
SHEET 2 OF 19 AUGUSTA, MAINE MARCH 1963

D. R. R. REG. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	I-95-7(18)	3	19

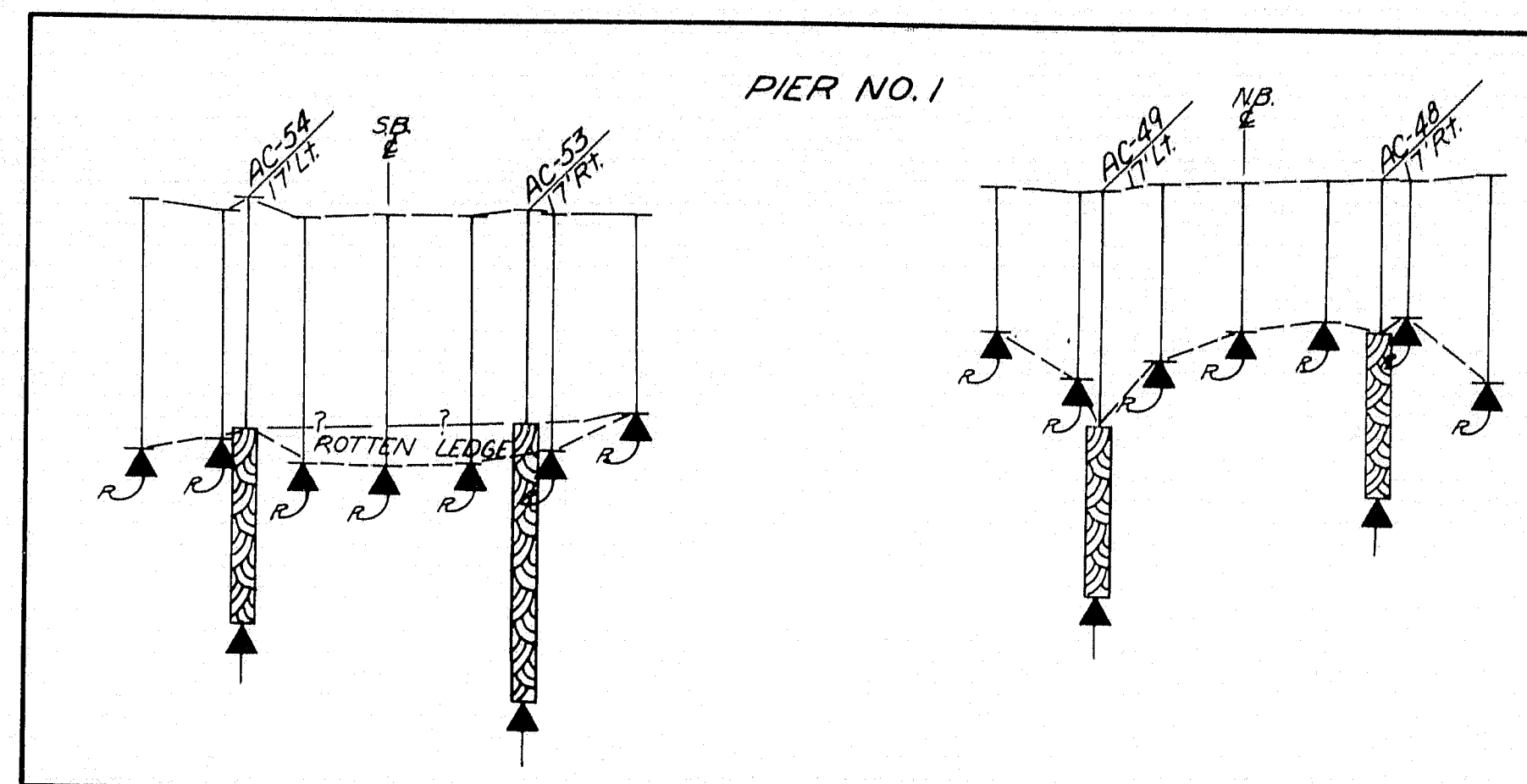


TRANSVERSE SECTIONS

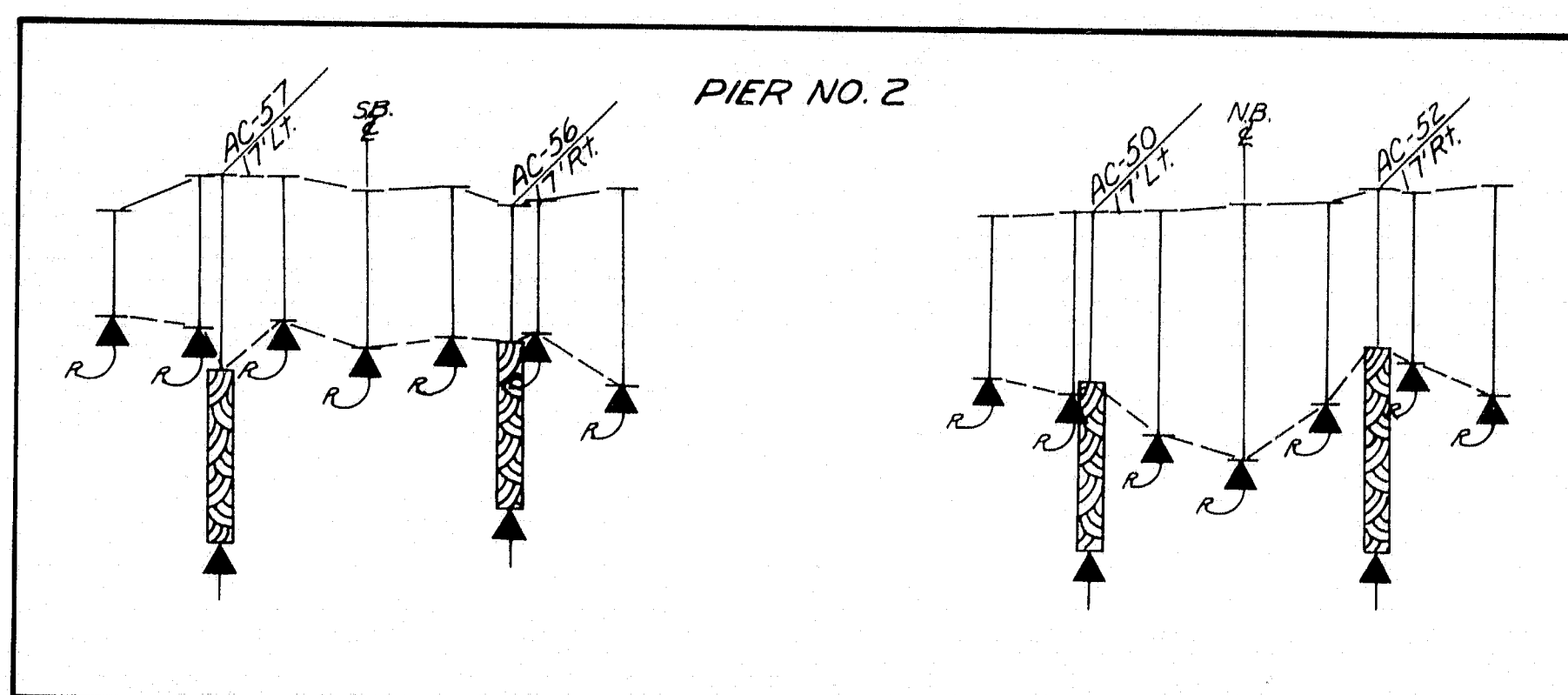
ABUTMENT NO. 1



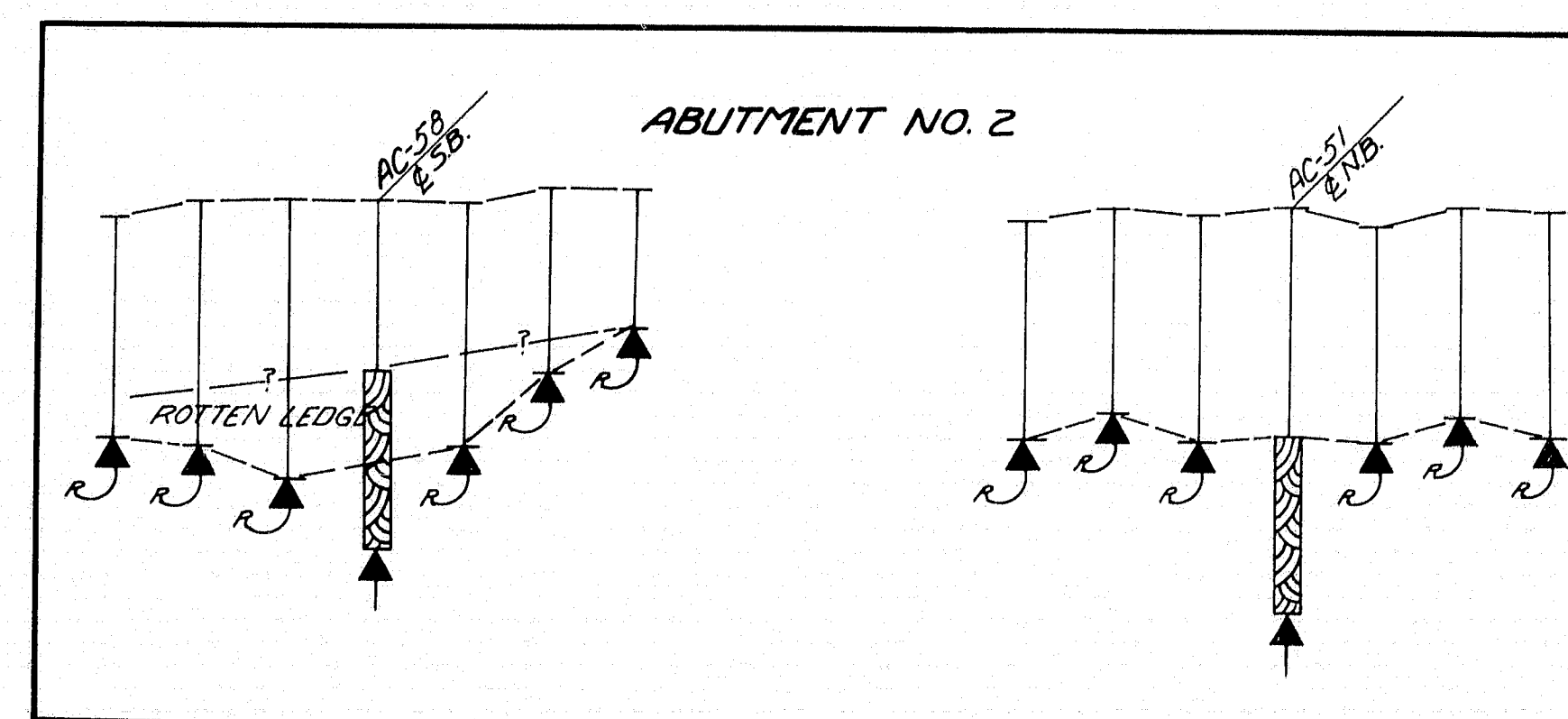
PIER NO. 1



PIER NO. 2

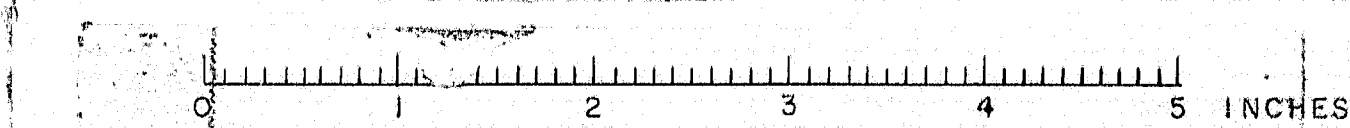


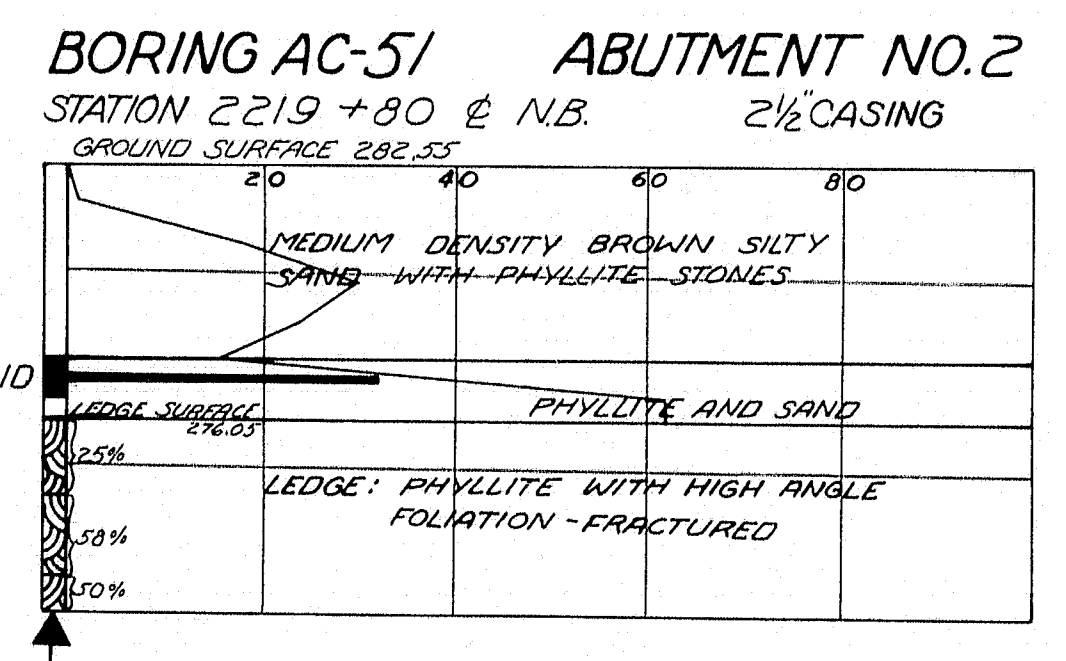
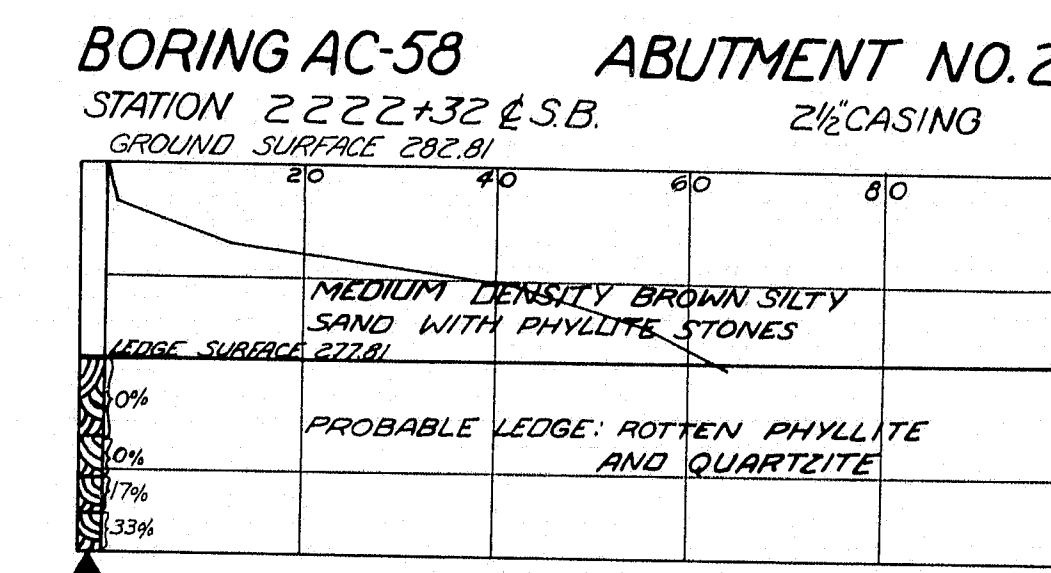
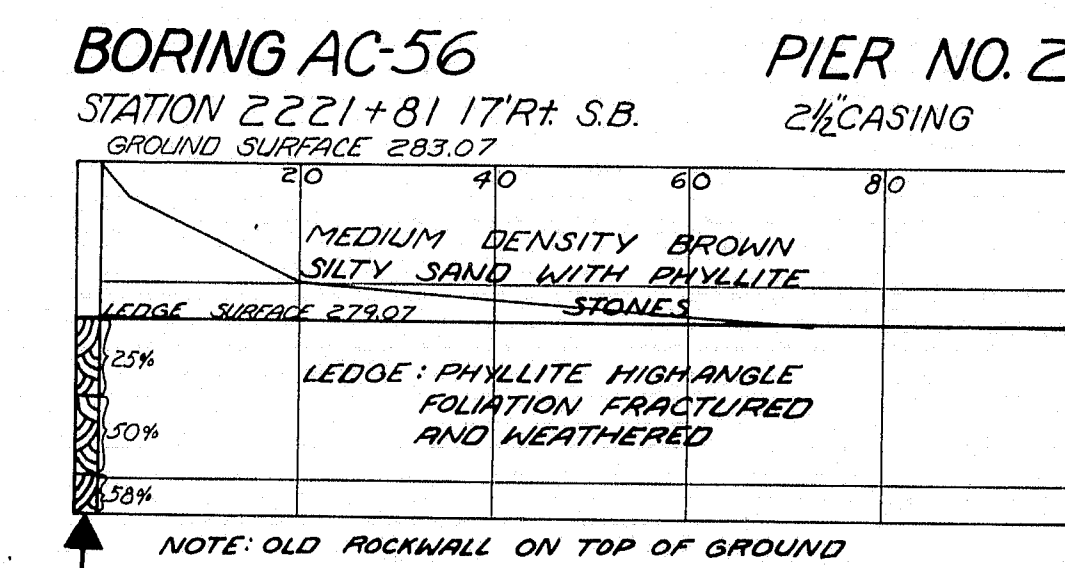
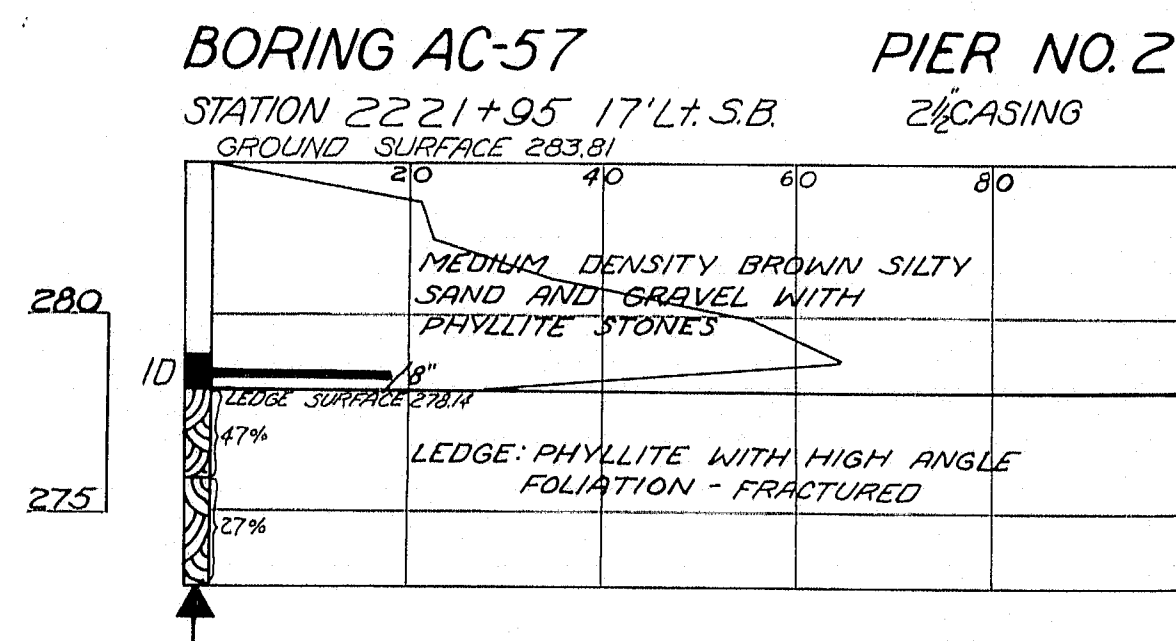
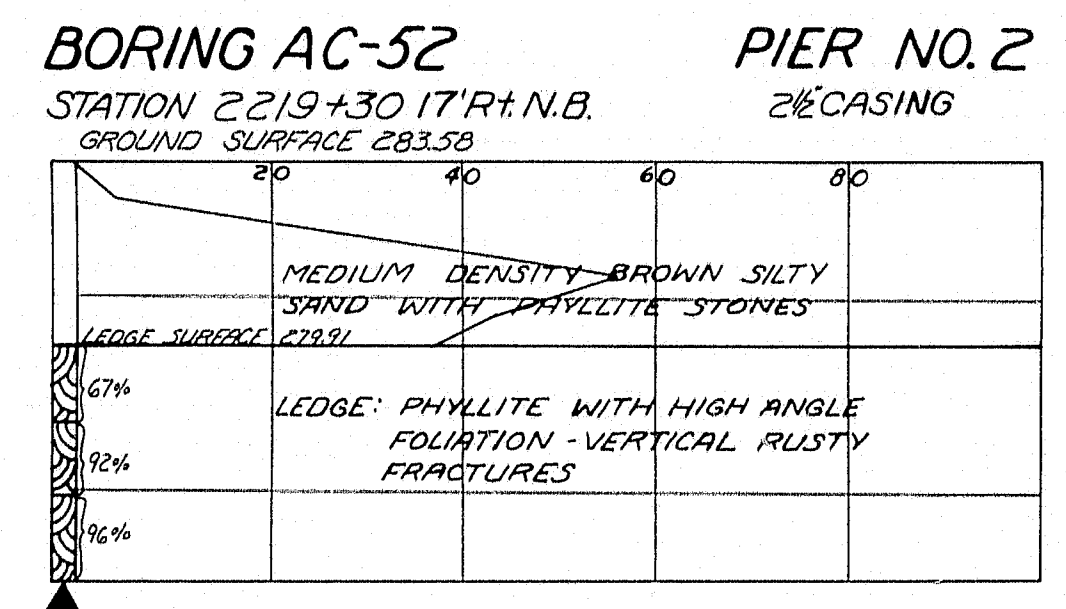
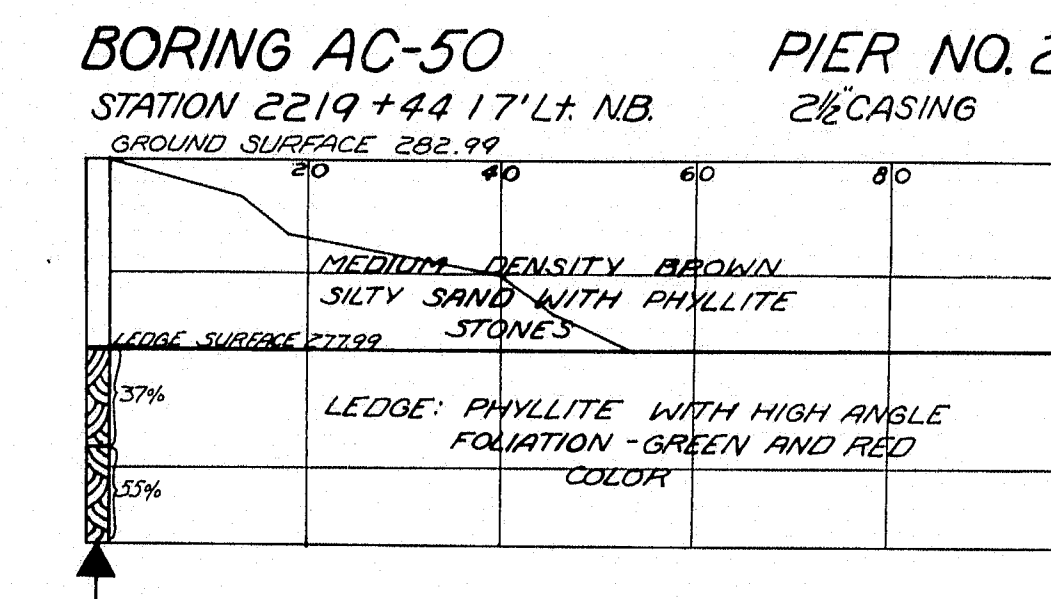
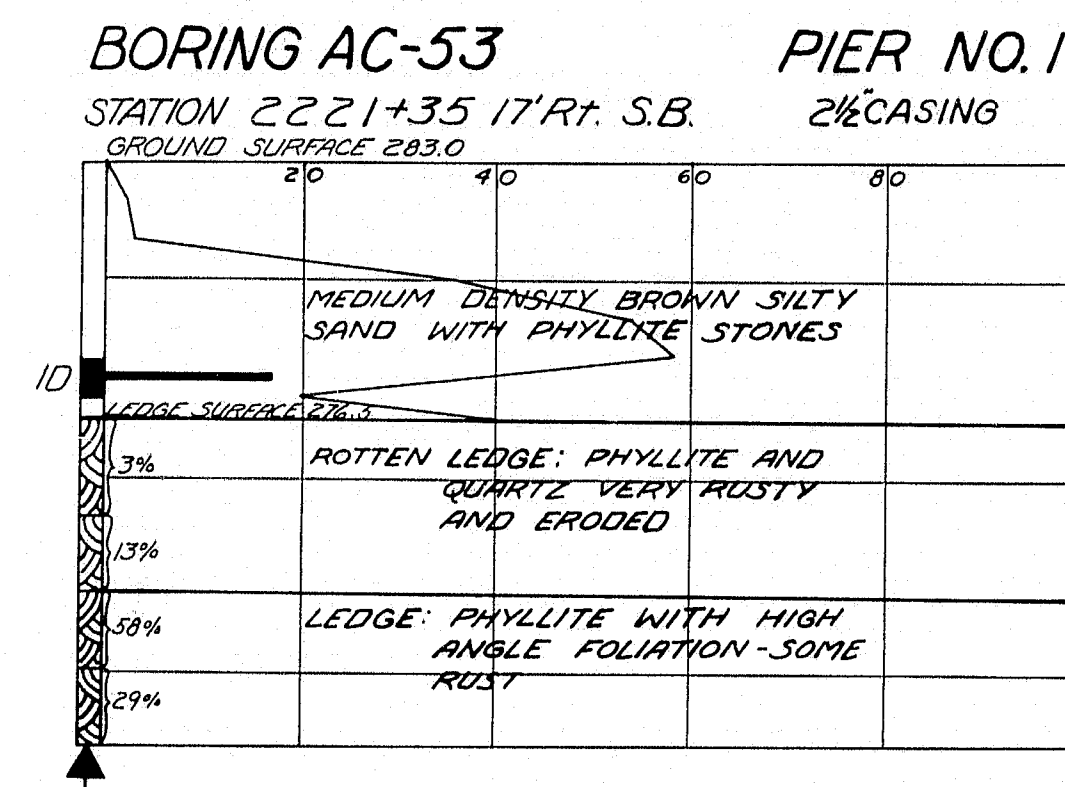
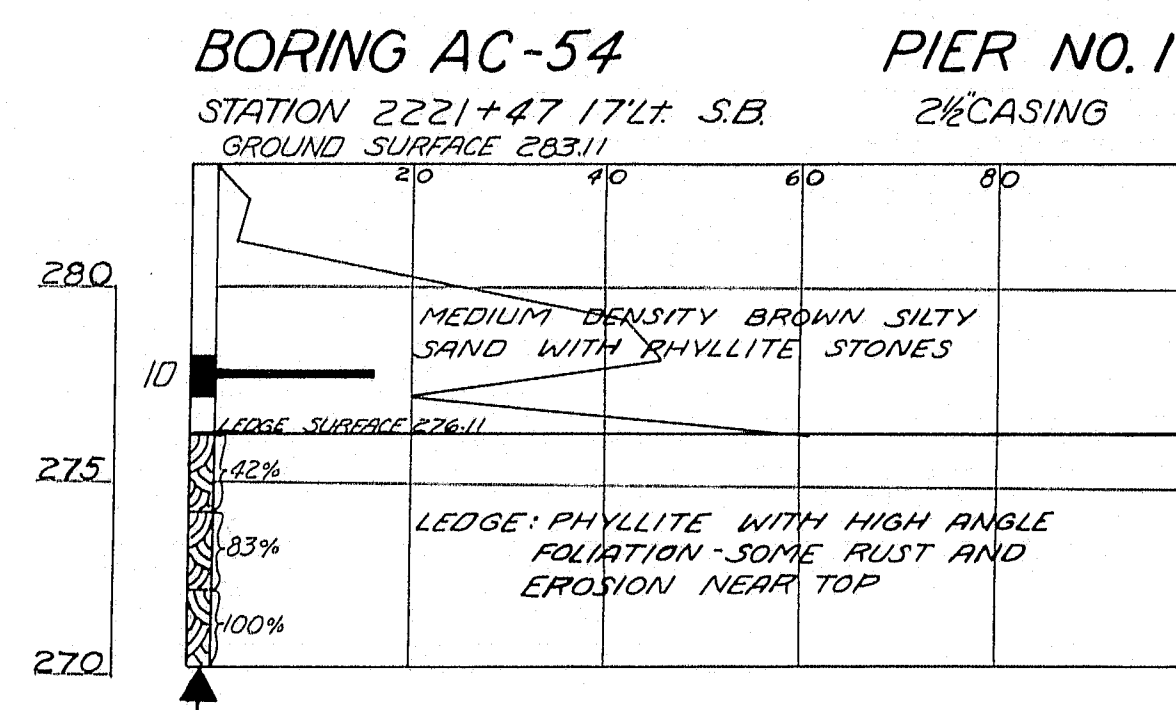
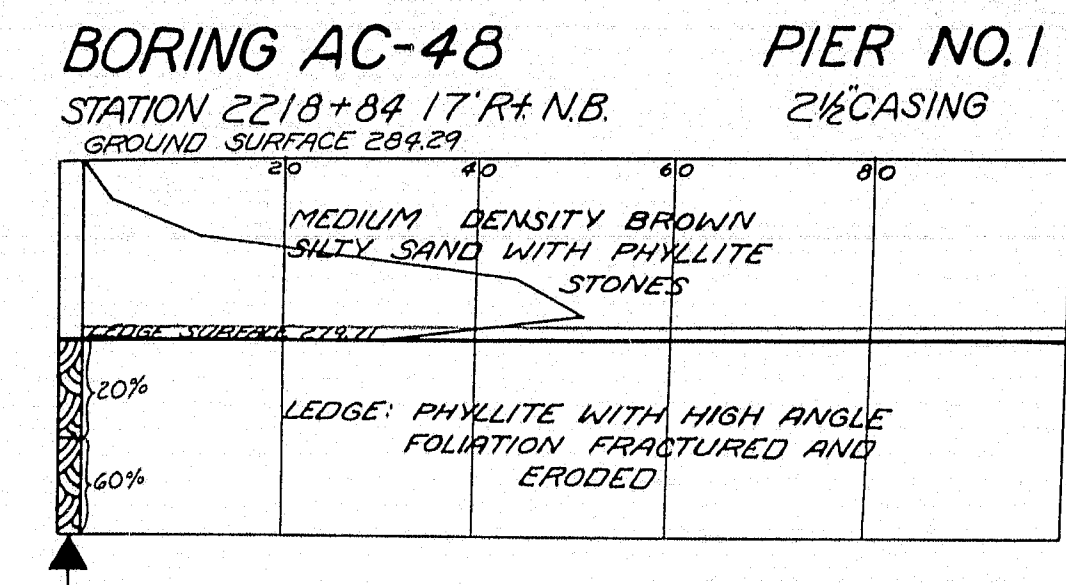
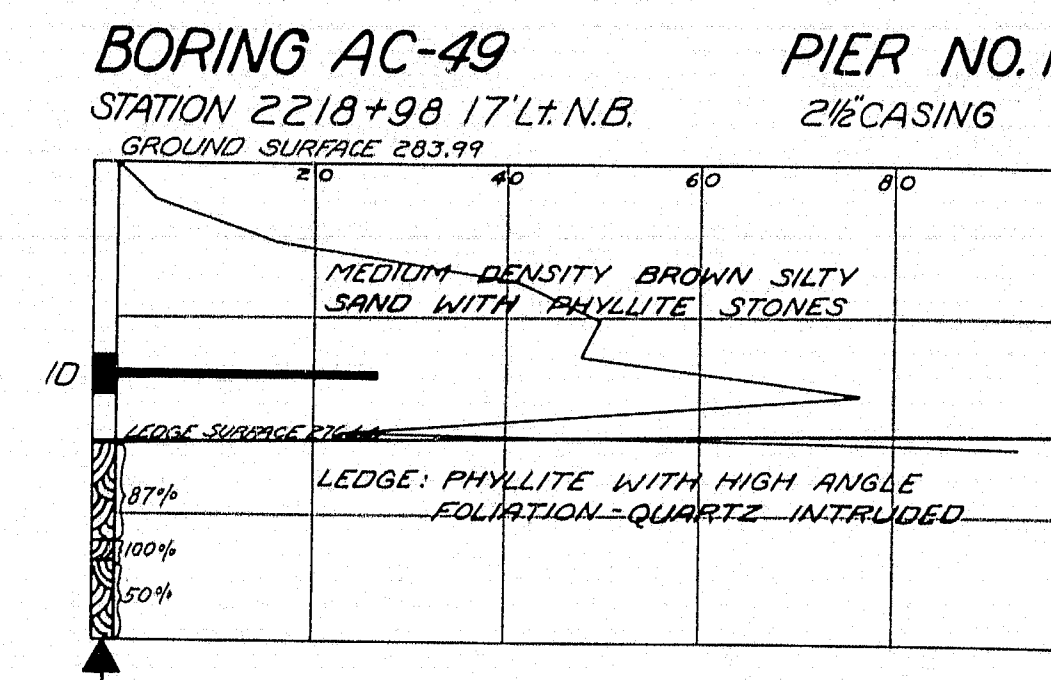
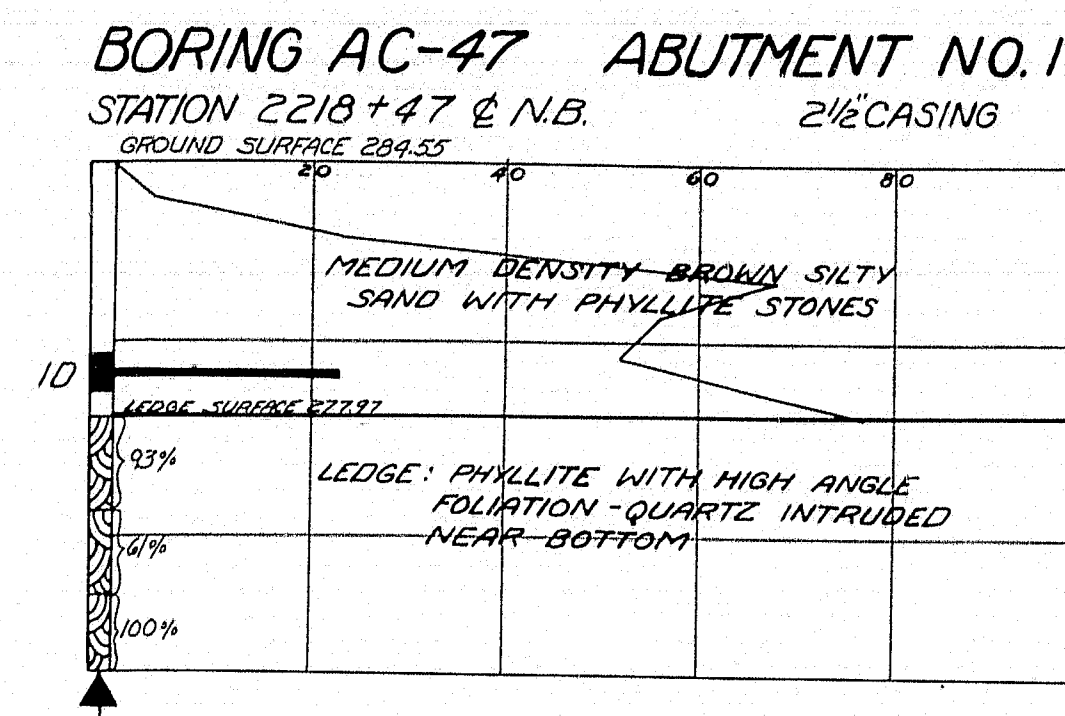
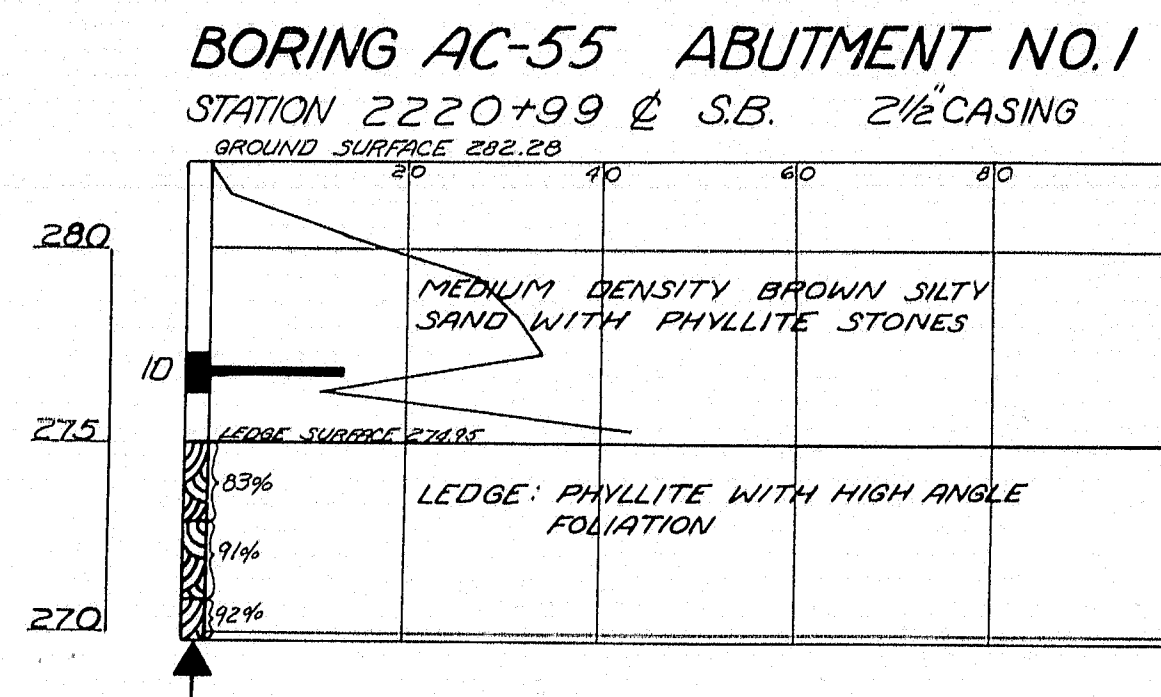
ABUTMENT NO. 2



SCALE VERTICAL 1"=5'
HORIZONTAL 1"=20'

DESIGN-7 TRACE-7 CHECK-7	SOILS LAB.	BRIDGE NO. SURVEY- PLOT-
STATE HIGHWAY COMMISSION BRIDGE DIVISION		
INTERSTATE 95 OVER NORTH MAIN STREET IN THE TOWN OF PITTSFIELD SOMERSET COUNTY FOUNDATION SURVEY		
SHEET 3 OF 19 AUGUSTA, MAINE, MARCH 1963		



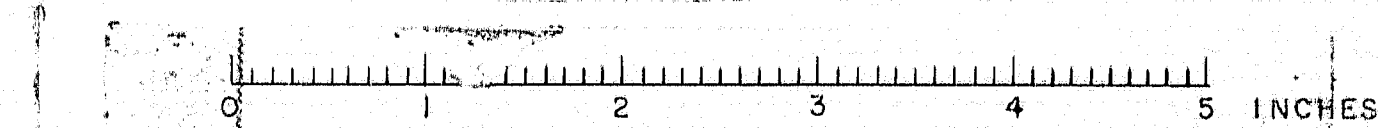


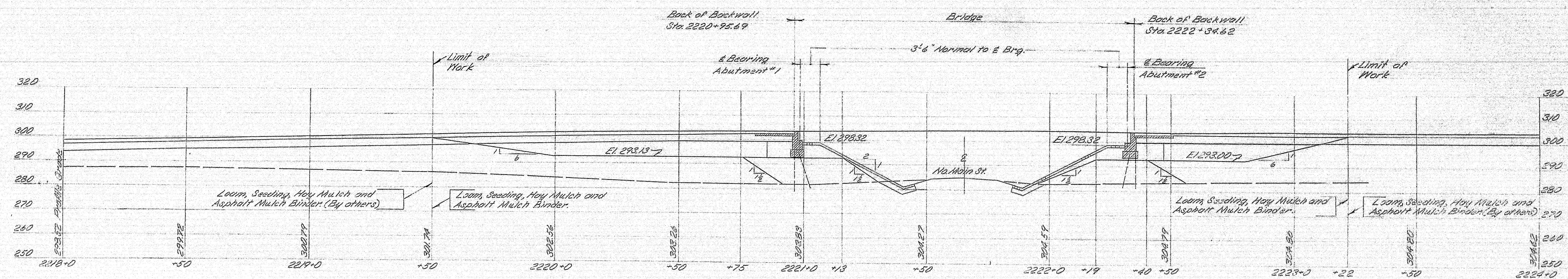
BORING NOTES

- ALL SAMPLES AND VANES ARE MADE AHEAD OF CASING
- NUMBER OF BLOWS REQUIRED TO DRIVE EXTRA HEAVY CASING ONE FOOT WITH 400 FT. LBS. OF ENERGY PER BLOW
- LOCATION OF SAMPLE OR SAMPLE ATTEMPT
- NUMBER AND TYPE OF DRY SAMPLE S&H SAMPLER #1290'S
- NUMBER OF BLOWS REQUIRED TO DRIVE SPOON OR TUBING ONE FOOT WITH 350 FT. LBS. OF ENERGY PER BLOW
- BOTTOM OF BORING (MAY NOT BE BOTTOM OF SOIL STRATA)
- LOCATIONS CORED BY DIAMOND BIT AND PER CENT RECOVERY OF ROCK

DRIVING RESISTANCE
BLOWS / FOOT

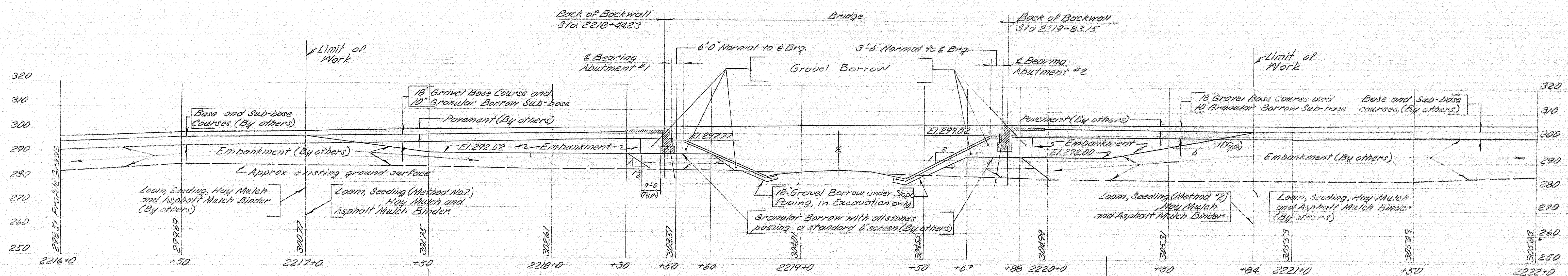
DESIGN - 7	BRIDGE NO.
TRACE - 7	SURVEY -
CHECK - 7	PLOT -
STATE HIGHWAY COMMISSION BRIDGE DIVISION	
INTERSTATE 95 OVER NORTH MAIN STREET IN THE TOWN OF PITTSFIELD SOMERSET COUNTY FOUNDATION - SURVEY	
SHEET 4 OF 19 AUGUSTA, MAINE MARCH 1963	





SECTION ALONG E INTERSTATE SB

All notes on NB section apply to this section except as noted.



SECTION ALONG E INTERSTATE NB

NOTES

See General Plan for ultimate location of berm lines, guard rails and pavement.

Before pits are driven, Granular Borrow and Gravel Borrow shall be placed and compacted to elevation of bottom of footing or substructure.

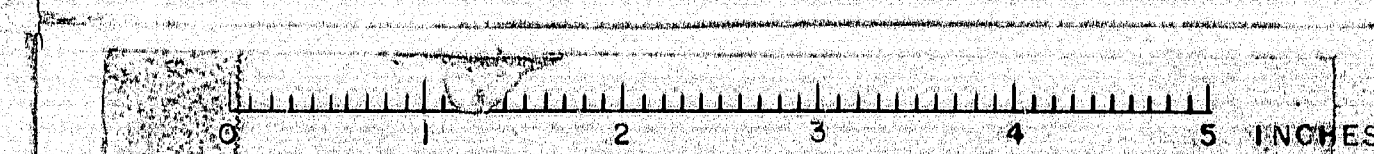
The controlled density method shall be used for placing embankment, granular, and Gravel Borrow under this contract.

The 18" gravel Borrow under Slope Paving may be reduced or omitted, if in the opinion of the Engineer, the existing material is suitable.

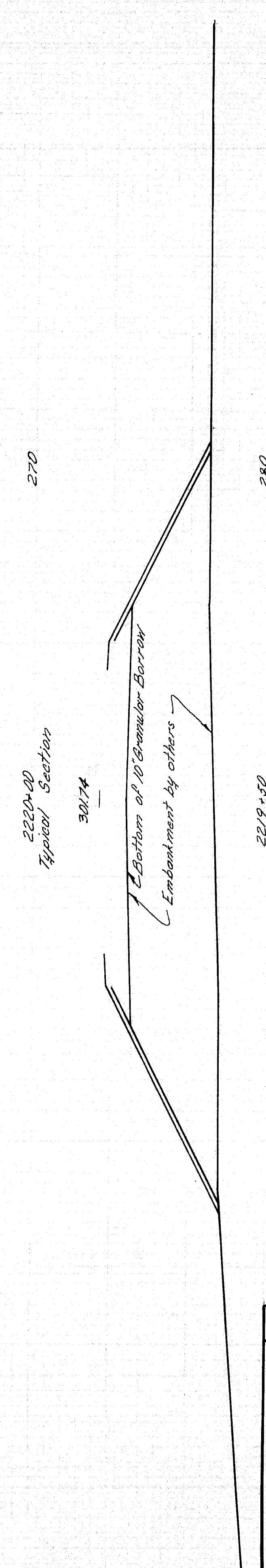
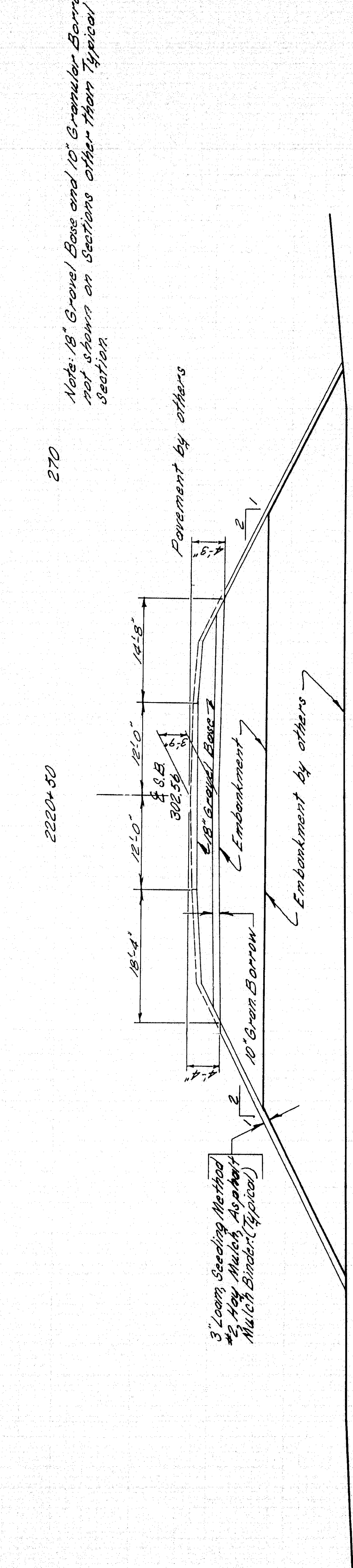
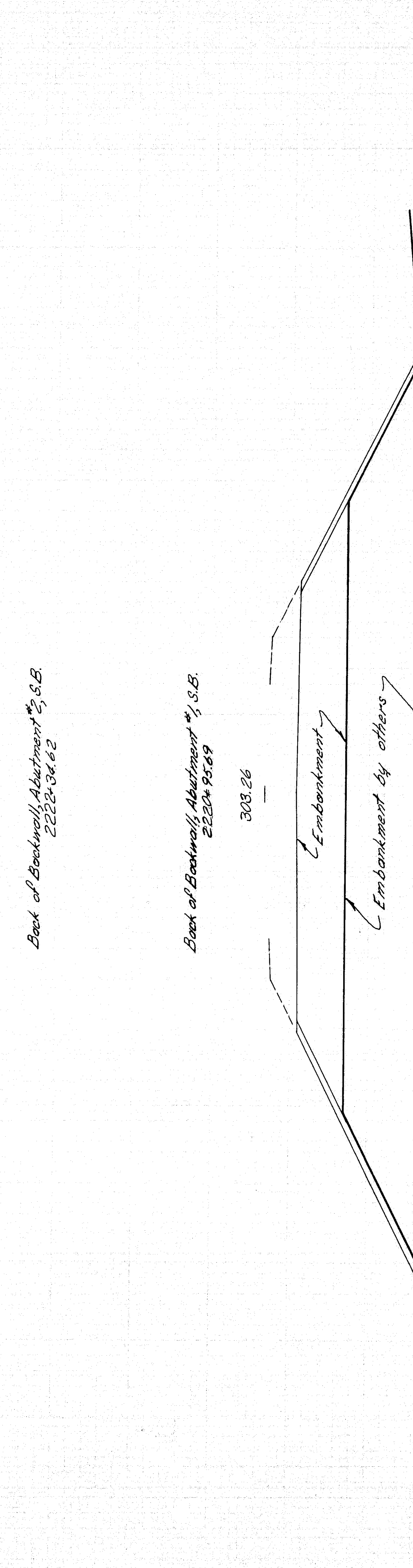
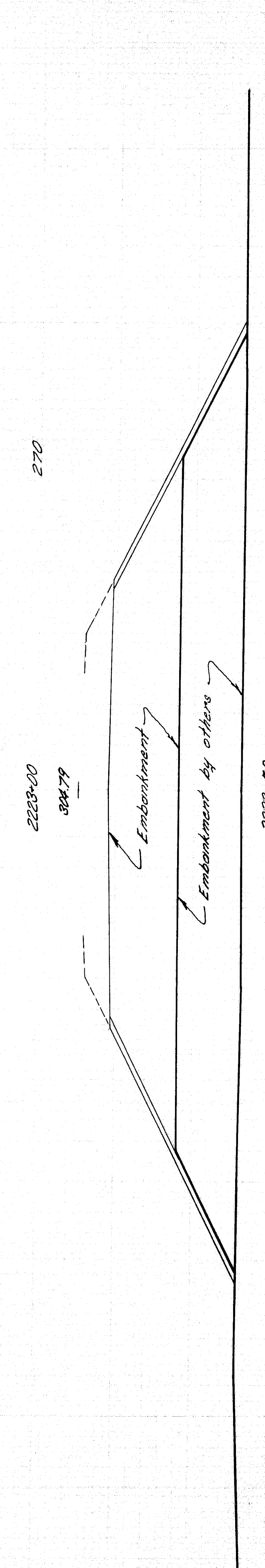
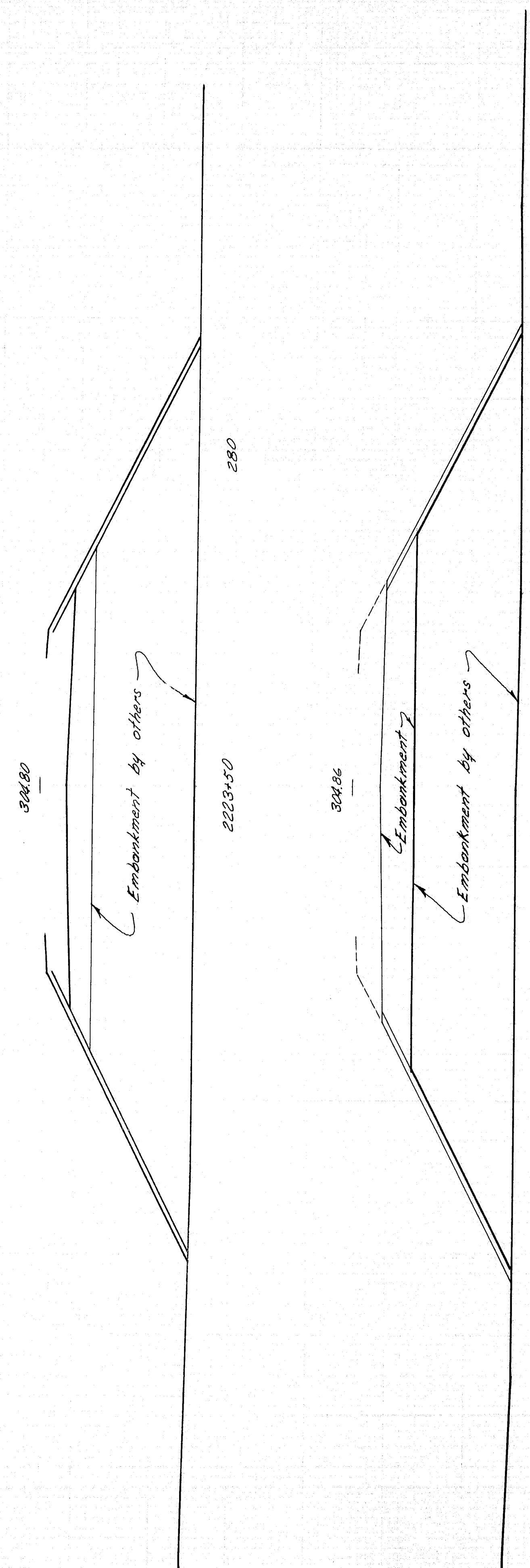
Payments for excavation for Gravel Borrow under Slope Paving to be made under Item 204-18, Structural Earth Excavation, Piers.

Guard rails to line up with inside face of concrete and posts on bridge.

DESIGN - T.H.K.	BRIDGE NO.
TRACE - CHACE	SURVEY -
CHECK - B.1016	PLOT -
STATE HIGHWAY COMMISSION BRIDGE DIVISION	
INTERSTATE 95	
OVER	
NORTH MAIN STREET	
IN THE TOWN OF	
PITTSFIELD	
SOMERSET COUNTY	
PROFILES - ROADWAY WORK	
SHEET 5 OF 19 AUGUSTA, MAINE MARCH 1963	



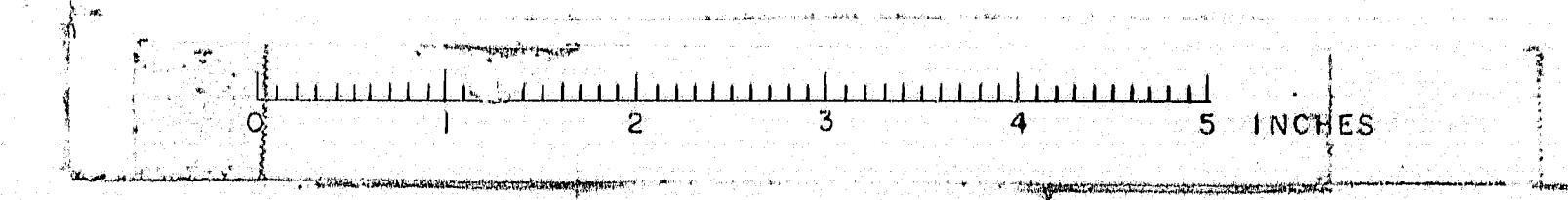
D. P. 16 REV. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	I-95-7(48)	6	19



Note: 18" Gravel Base and 10" Granular Base not shown on sections other than typical section

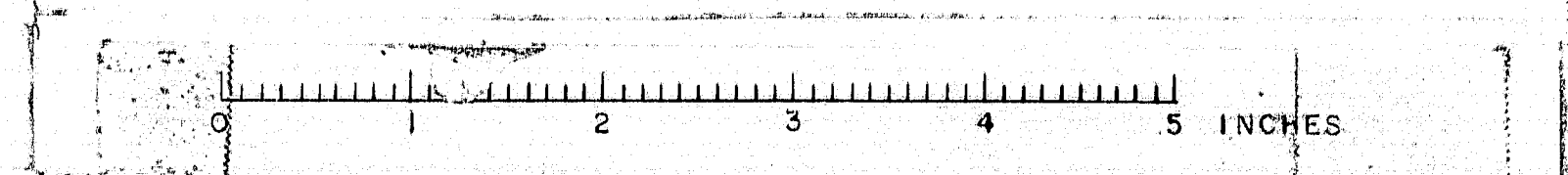
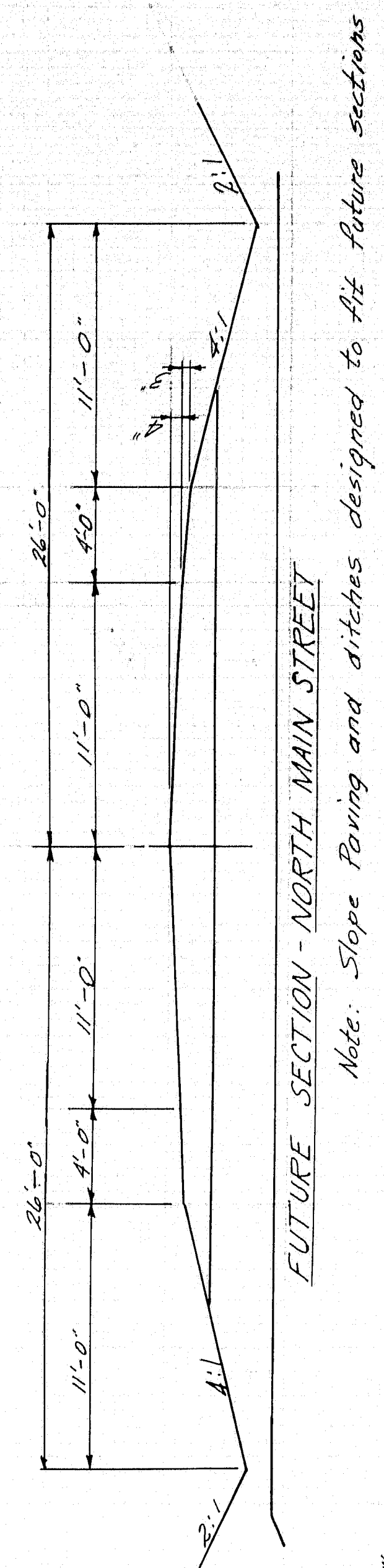
3' Loam Seeding Method
2" Hay Mulch Layer
Match Bladder (typical)

DESIGN - T.H.K. DETAIL - CHACE CHECK - Blake	STATE HIGHWAY COMMISSION BRIDGE DIVISION INTERSTATE 95 OVER NORTH MAIN STREET IN THE TOWN OF PITTSFIELD SOMERSET COUNTY SOUTH BOUND CROSS SECTIONS SHEET 6 OF 19 AUGUSTA, MAINE MARCH 1963
--	---



E.S.B.

B. P. R. REG. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	I-95-7(48)	8	19



TRACE CHECK	FILED <i>11/26</i>	BRIDGE NO. PLOT	FILED <i>11/26</i>
----------------	-----------------------	--------------------	-----------------------

STATE HIGHWAY COMMISSION
BRIDGE DIVISION

INTERSTATE 95
OVER

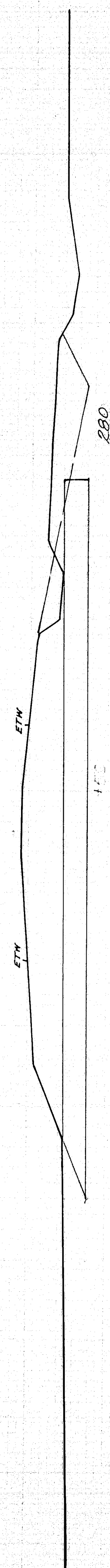
NORTH MAIN STREET
IN THE TOWN OF
PITTSFIELD
SOMERSET COUNTY

CROSS SECTIONS
NORTH MAIN ST.

SHEET 8 OF 19 AUGUSTA, MAINE, MAY 1963

D. P. R. REG. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	2-98-7(40)	9	19

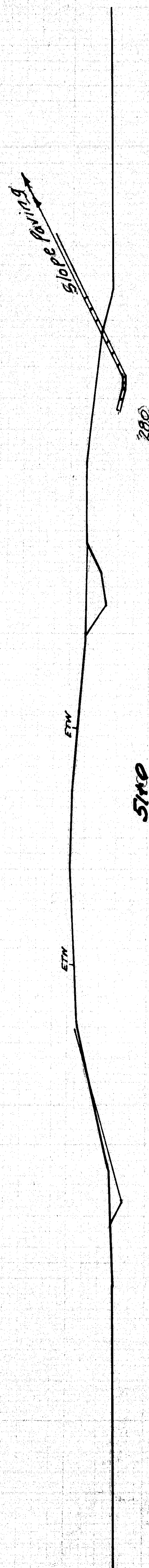
ETW = Edge of Traveled Way



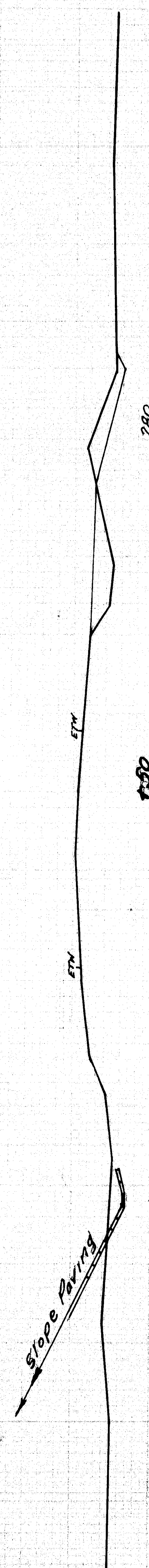
Sho. 31+50
18' x 60' ACCMP to be installed by others



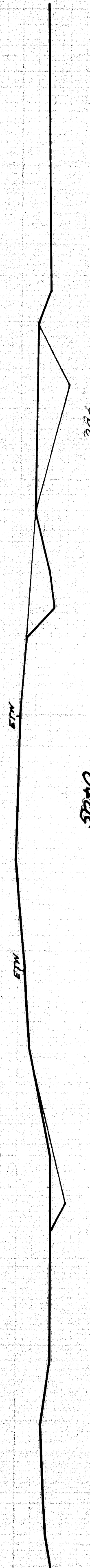
x07
Existing 15' x 32' ACCMP to be removed by others



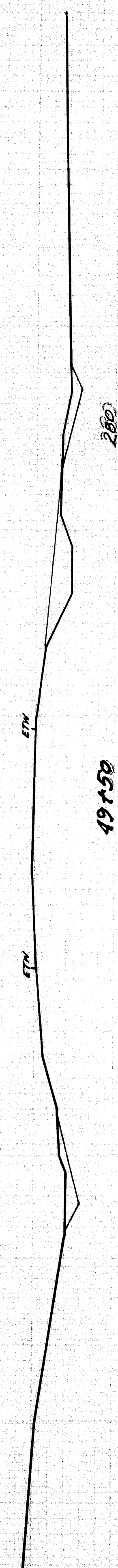
5MB



x00

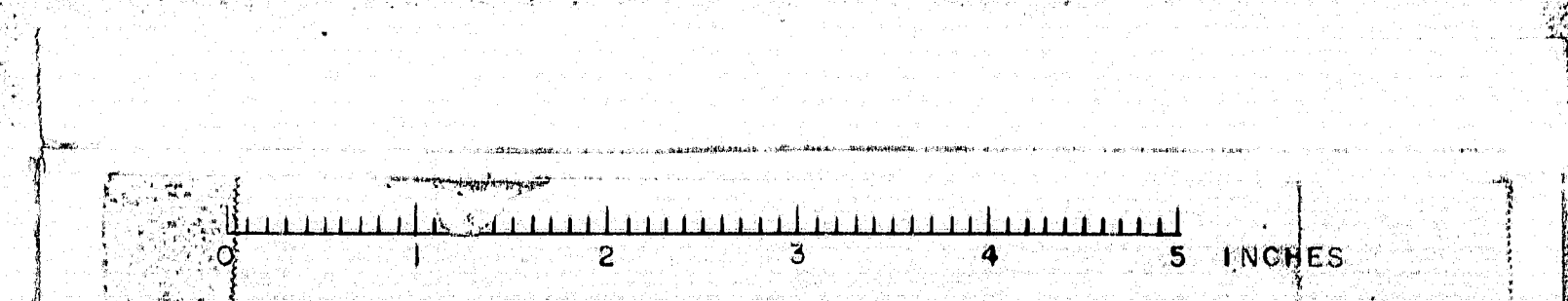


5000

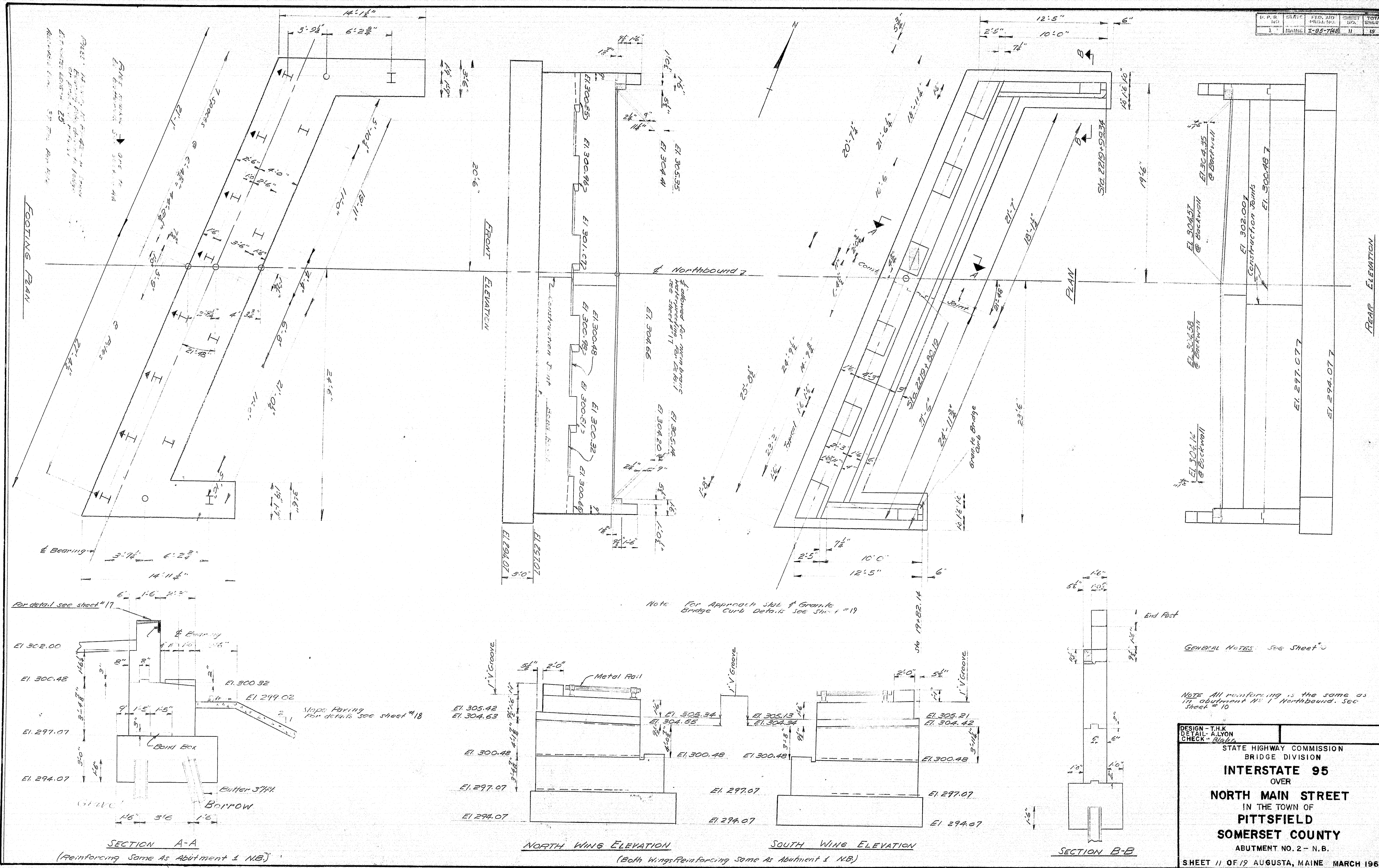


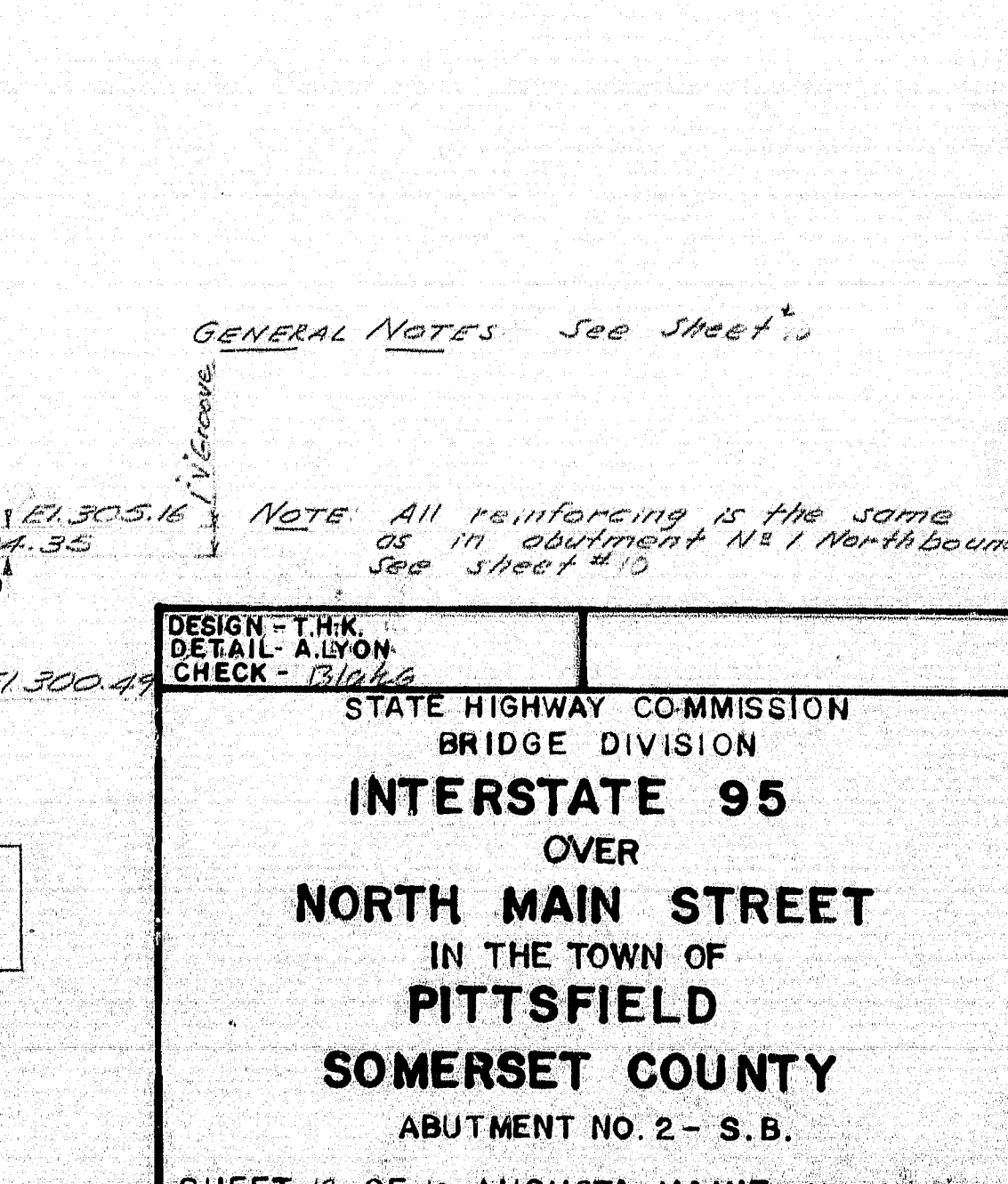
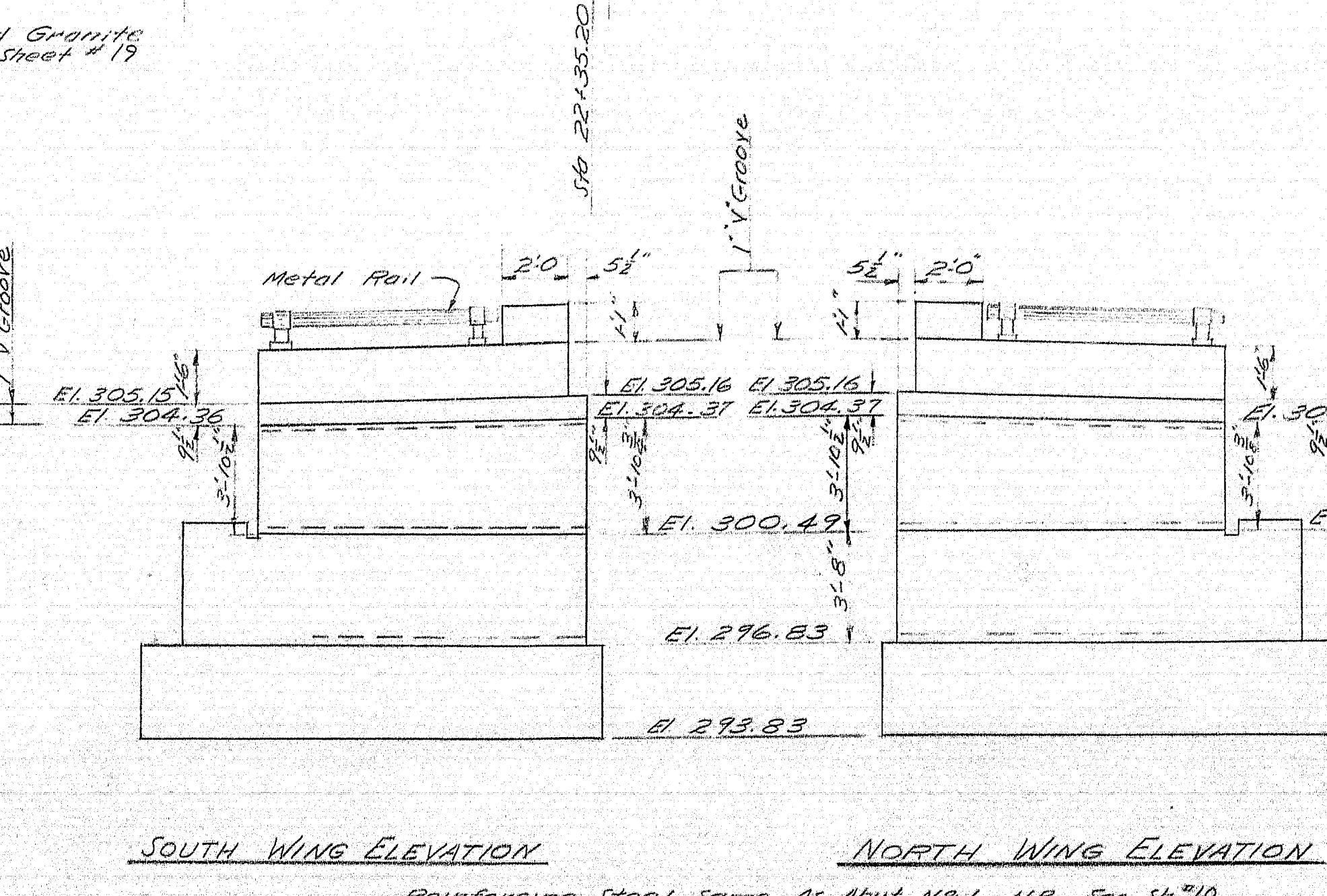
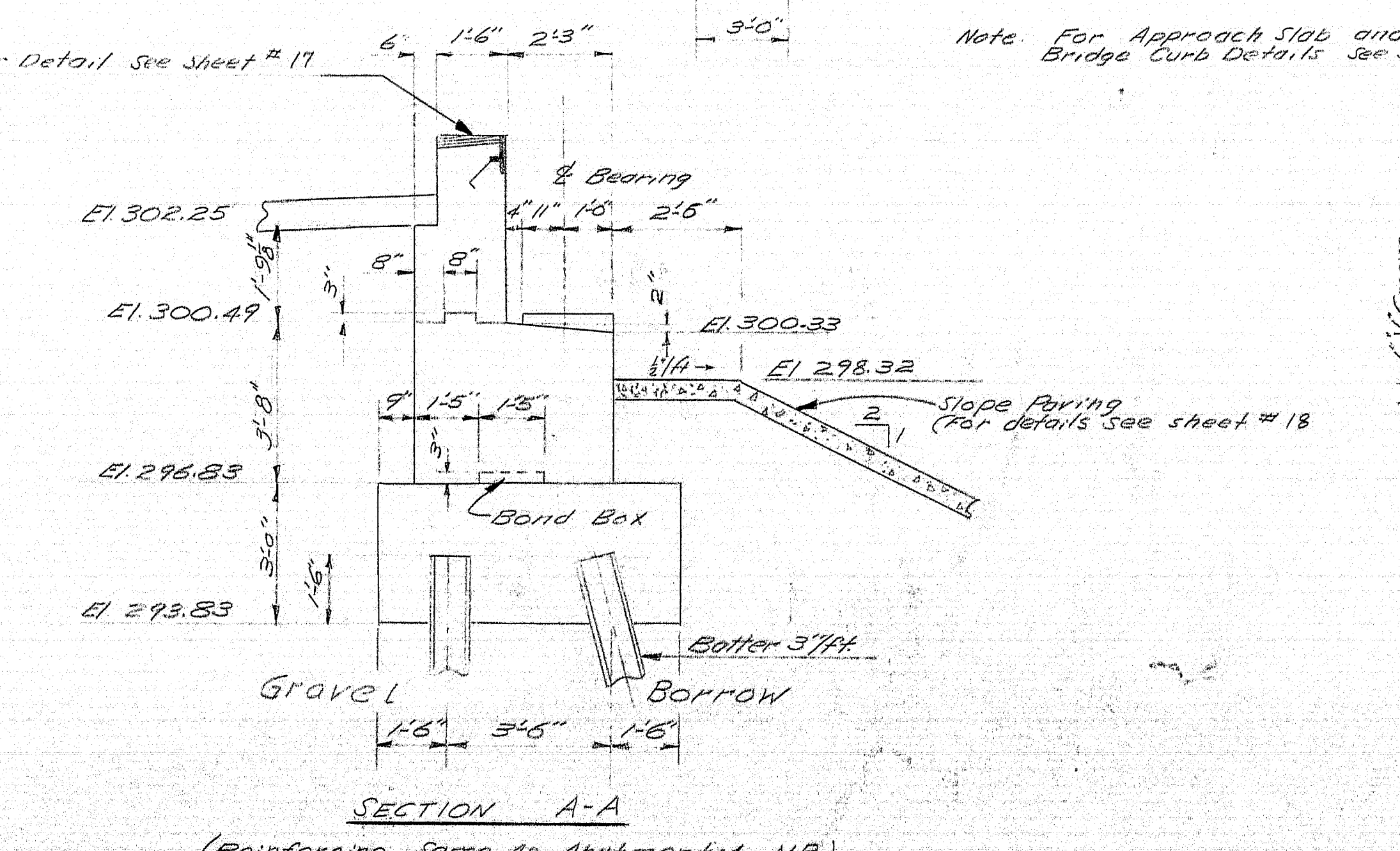
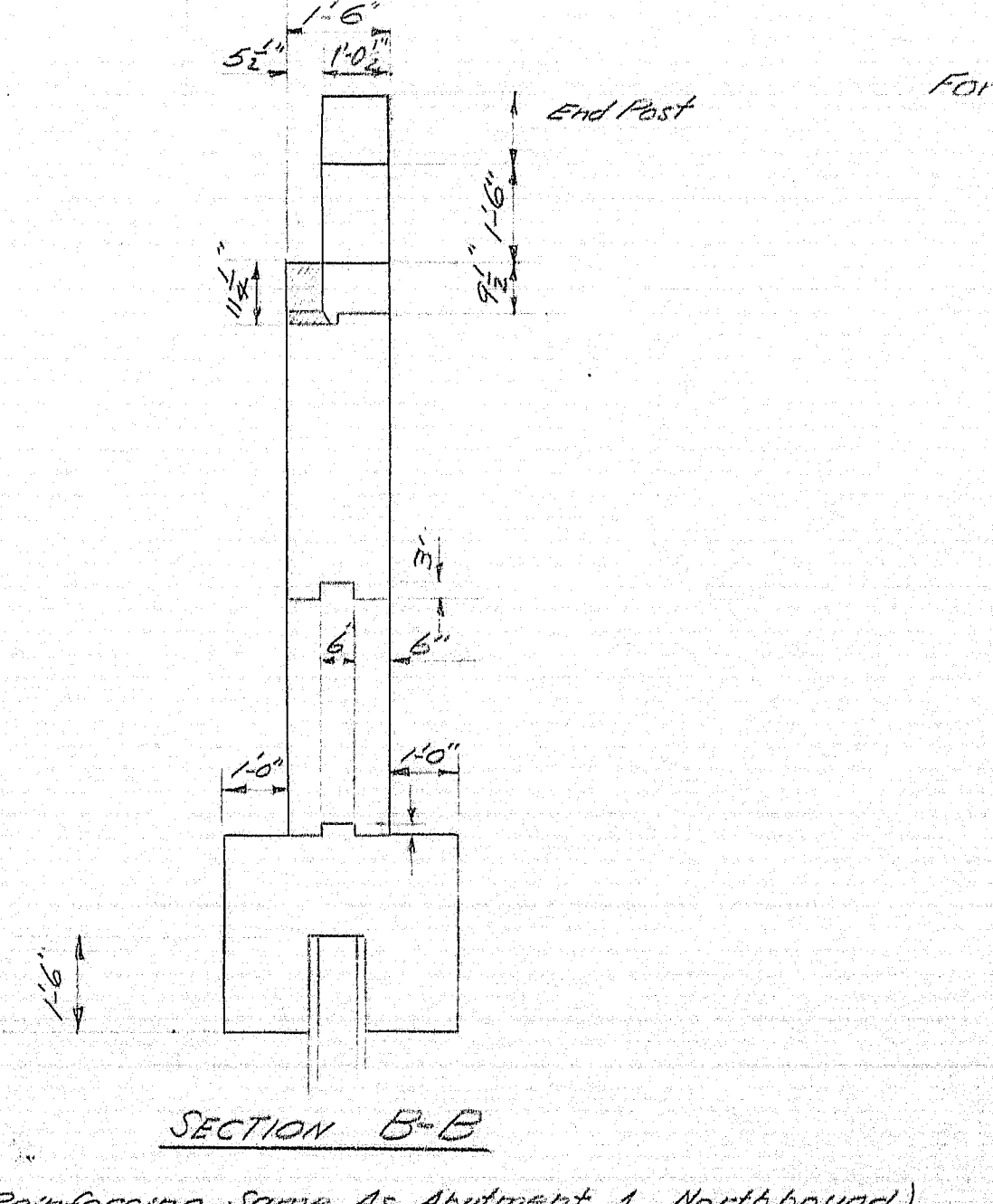
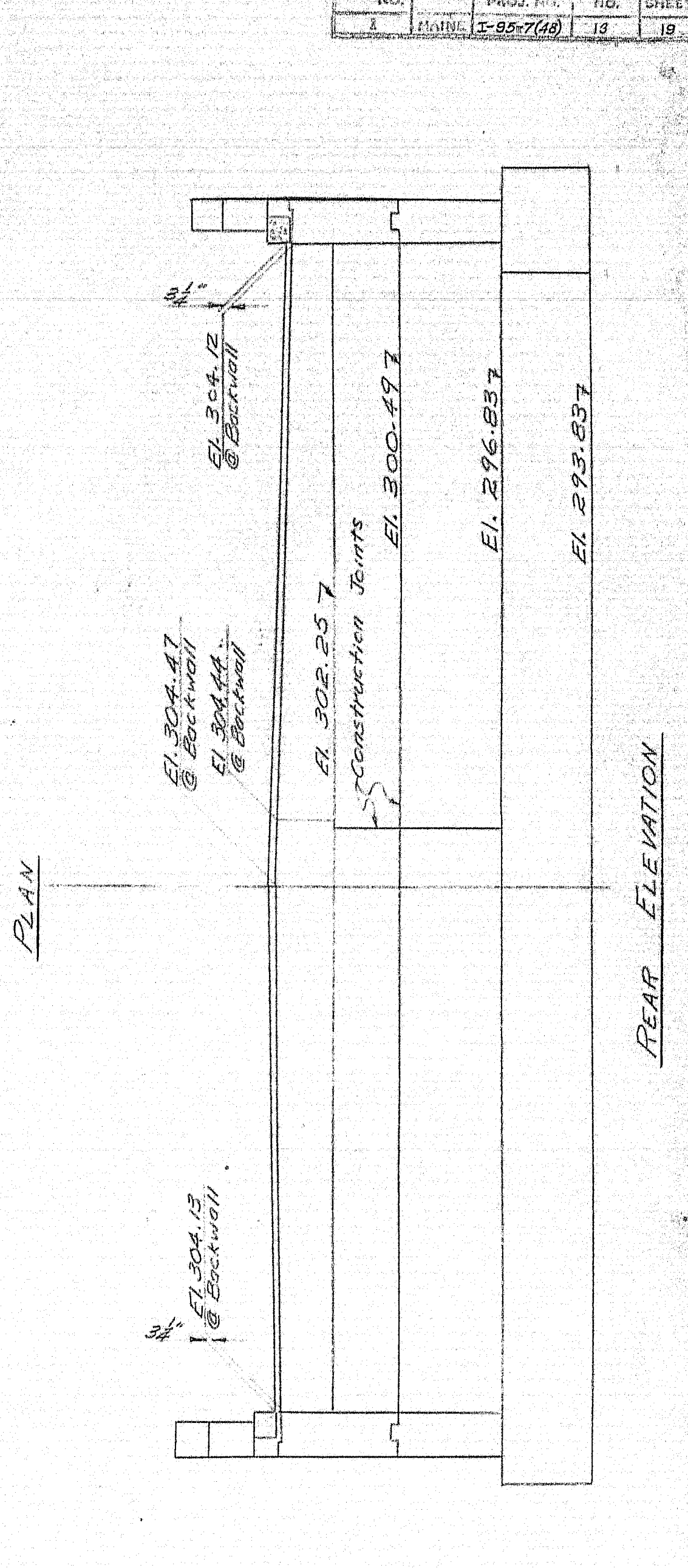
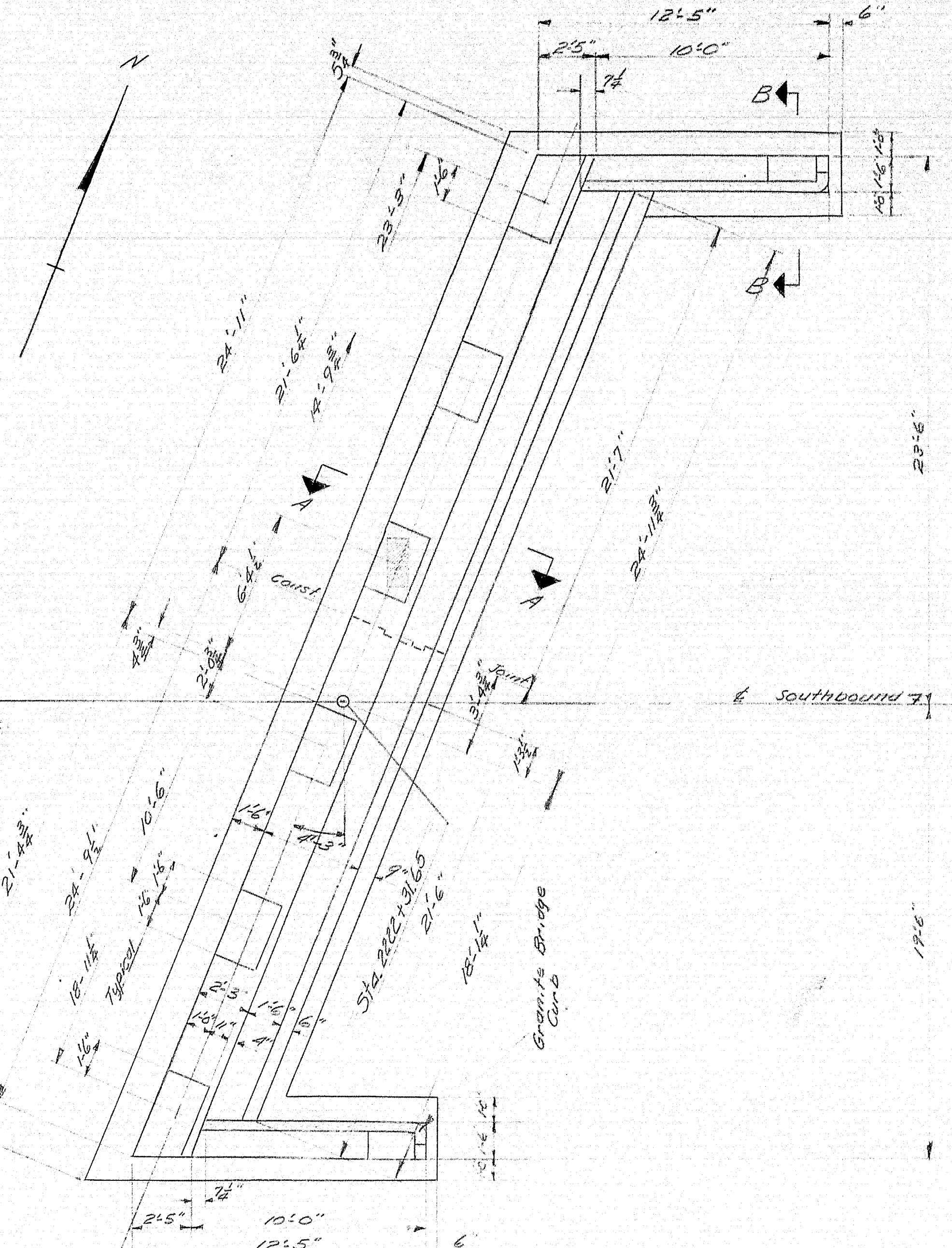
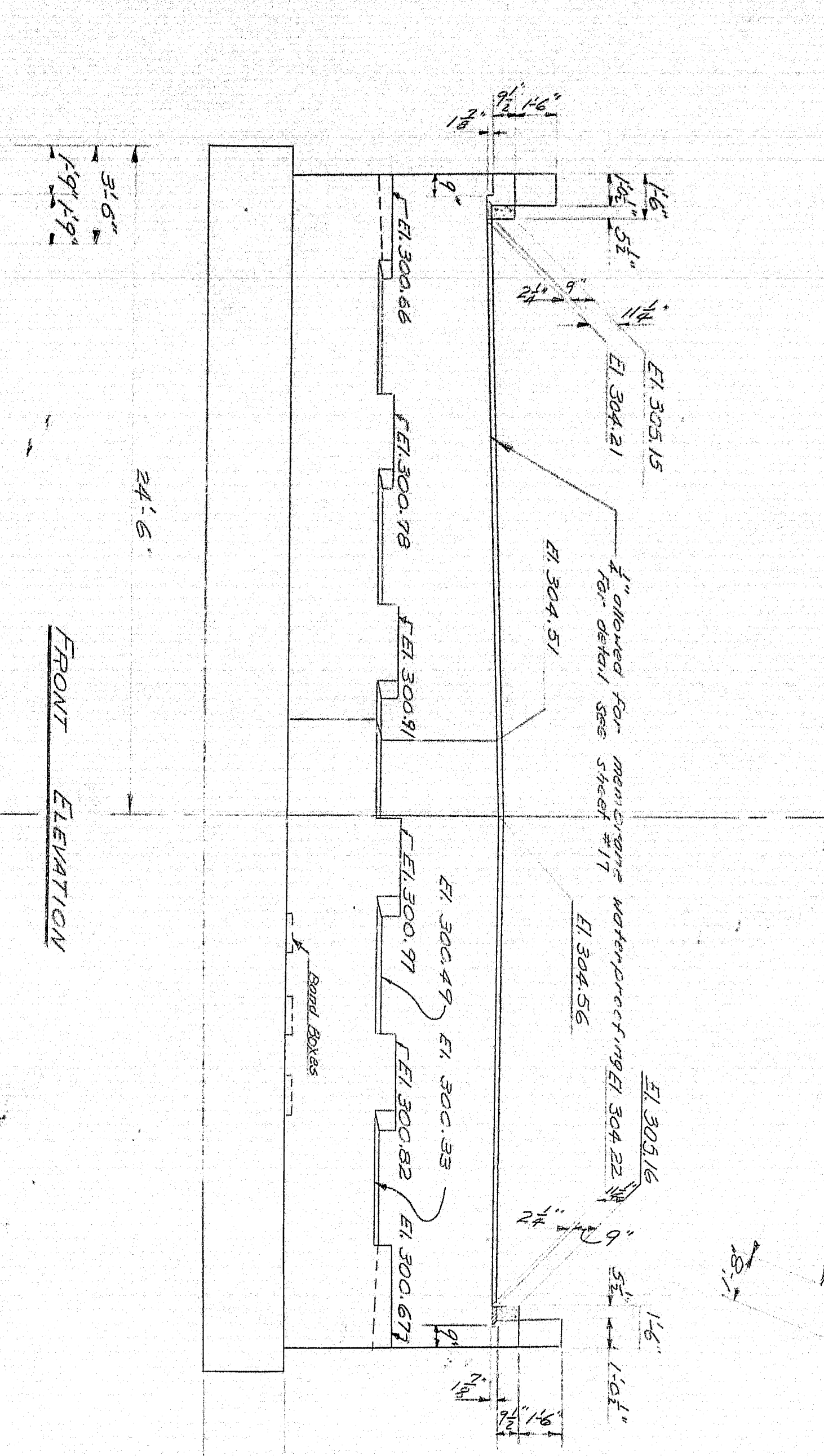
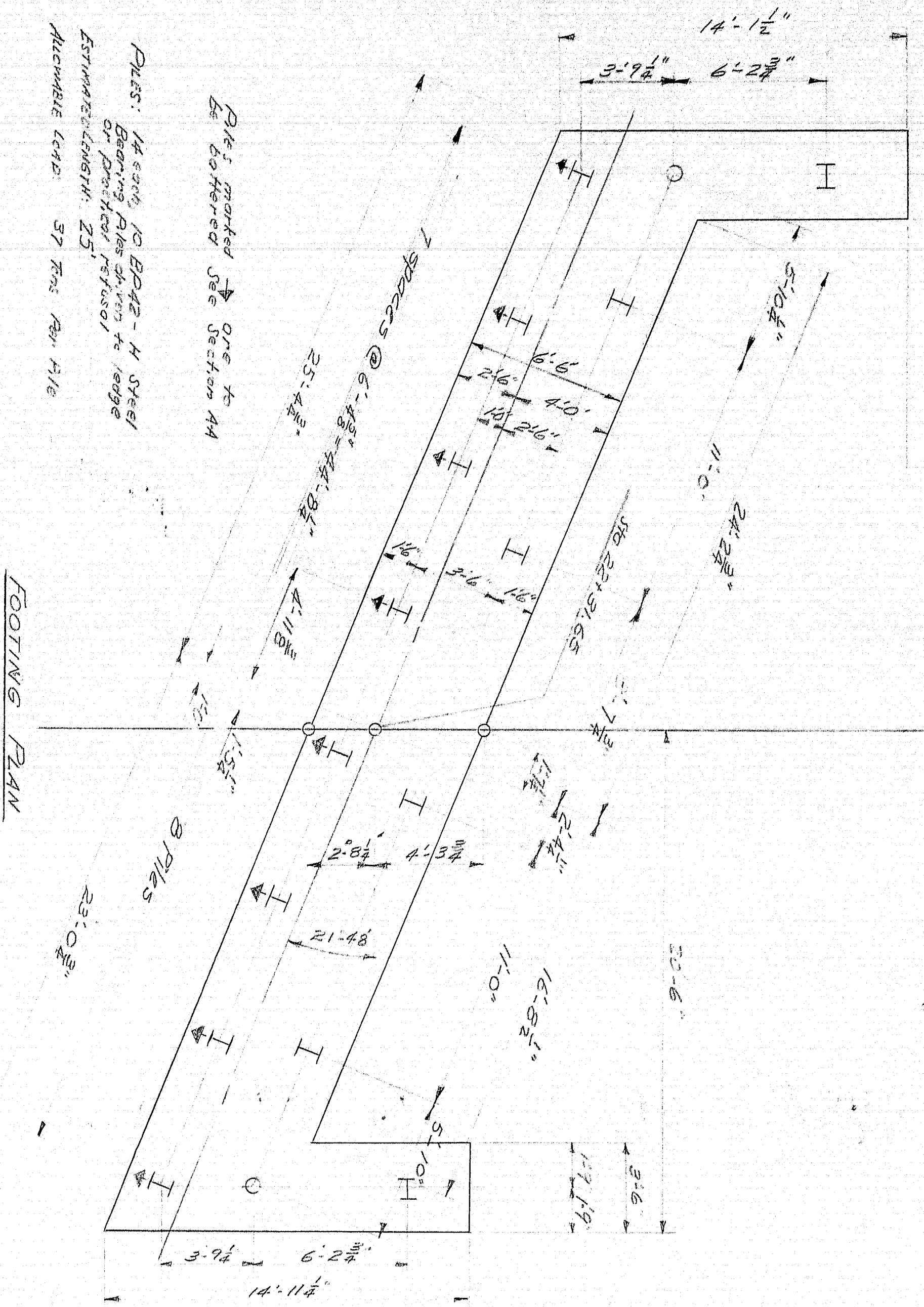
19+50

Future E North Main St.



DESIGN TRACE - P.L.A. CHECK 8/10/66	BRIDGE NO.
STATE HIGHWAY COMMISSION BRIDGE DIVISION	
INTERSTATE 95 OVER	
NORTH MAIN STREET IN THE TOWN OF PITTSFIELD SOMERSET COUNTY	
CROSS SECTIONS NORTH MAIN ST.	
SHEET 9 OF 19 AUGUSTA, MAINE JANUARY 1963	





GENERAL NOTES See Sheet #1

NOTE All reinforcing is the same as in abutment No. 1 Northbound See Sheet #10

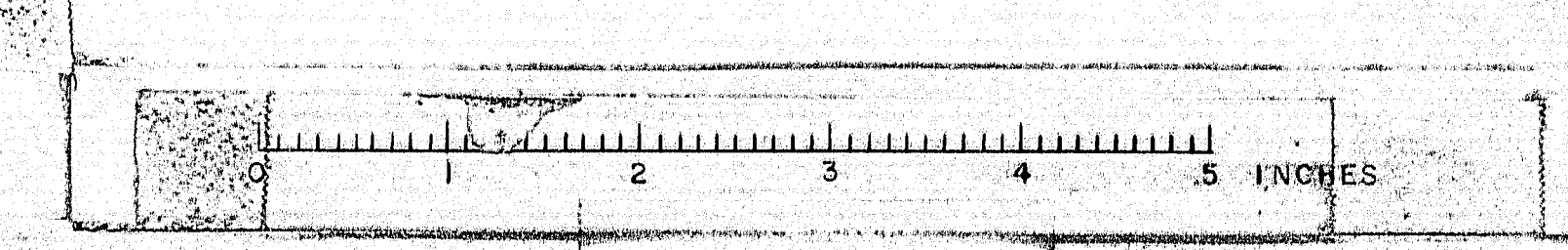
DESIGN - T.H.K.
DETAIL - A.L.YON
CHECK - B.L.A.

STATE HIGHWAY COMMISSION
BRIDGE DIVISION

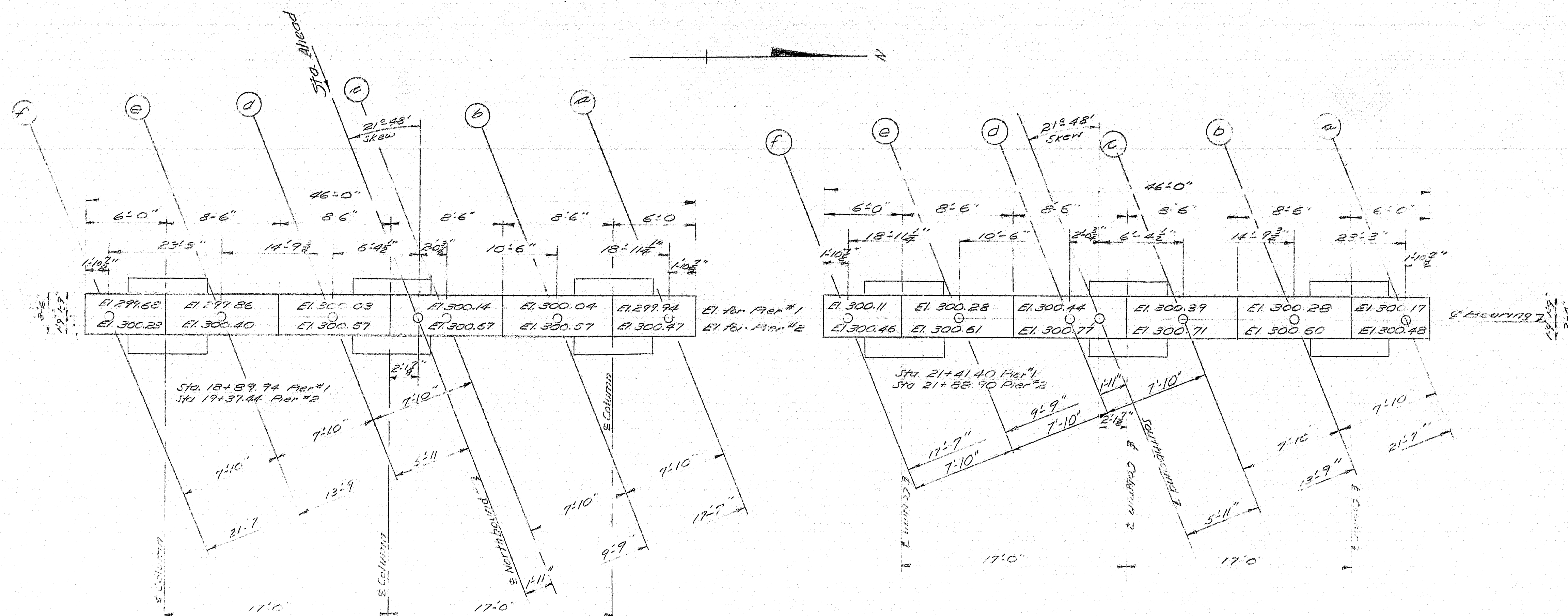
INTERSTATE 95
OVER
NORTH MAIN STREET
IN THE TOWN OF
PITTSFIELD
SOMERSET COUNTY
ABUTMENT NO. 2 - S.B.

SHEET 13 OF 19 AUGUSTA, MAINE MARCH 1963

86-194



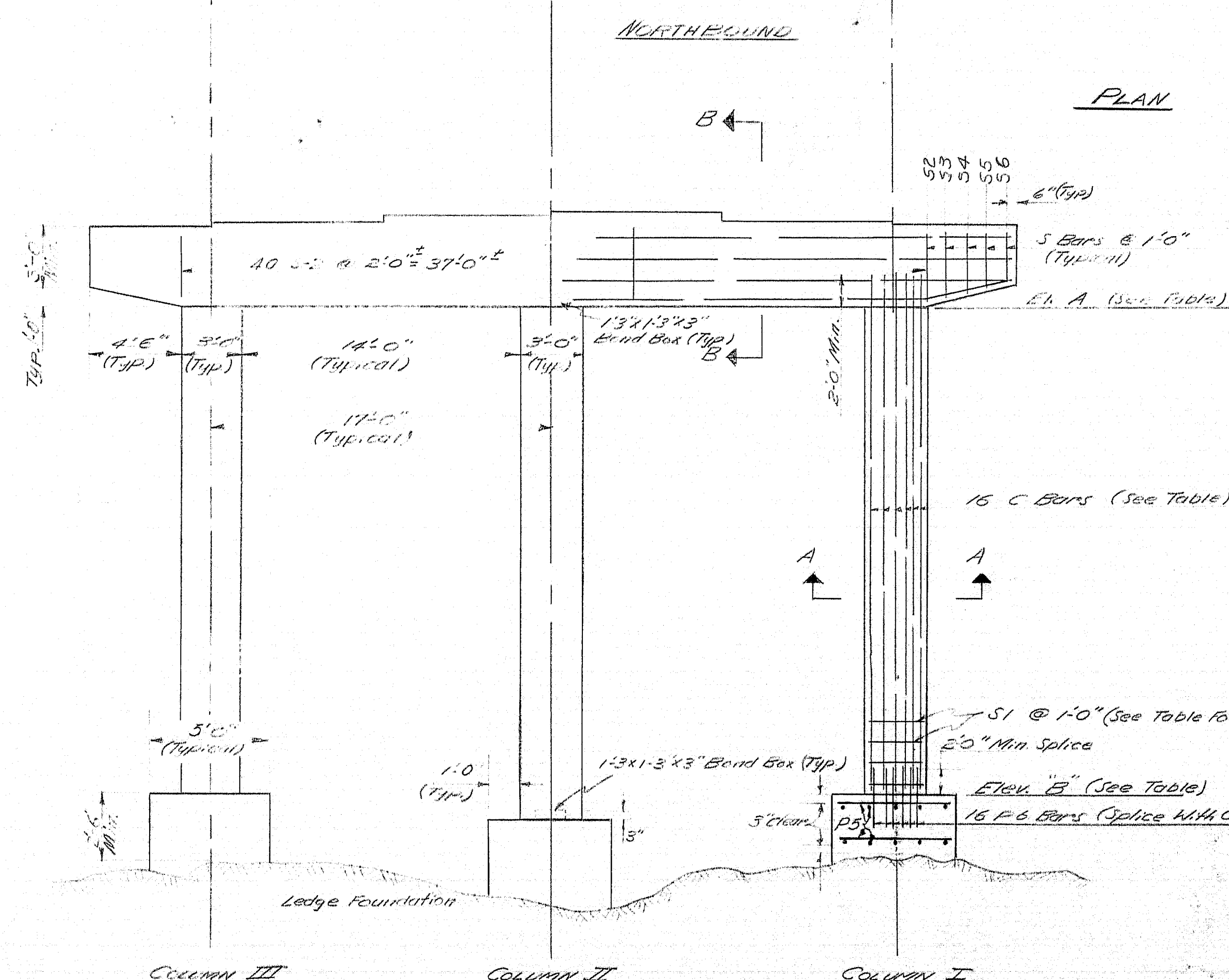
GENERAL NOTES:
 All weathered and/or broken ledge to be removed before any footing concrete is placed.
 Dress bearing areas 1' larger, all around, than masonry piers, and to exact elevations shown. Caulk around edges of masonry piers with an approved caulking material. Payment to be incidental to contract items.
 If sound ledge is encountered and excessive rock excavation would be required to obtain a 2'-6" minimum depth of footing, the elevation for the top of any footing may be raised. However, the top of the footing shall not be less than 1'-0" from the surface of the final ground elevation of the pier in question.
 In the case of overbreakage of ledge no payment will be made for rock excavation, concrete and cement of depths more than 6" below the elevation of the bottom of any column footing as previously determined by the Engineer.
 Minimum footing pressure = 14 tons / sq. ft.
 Reinforcing steel to have 2' Min. cover unless otherwise shown.



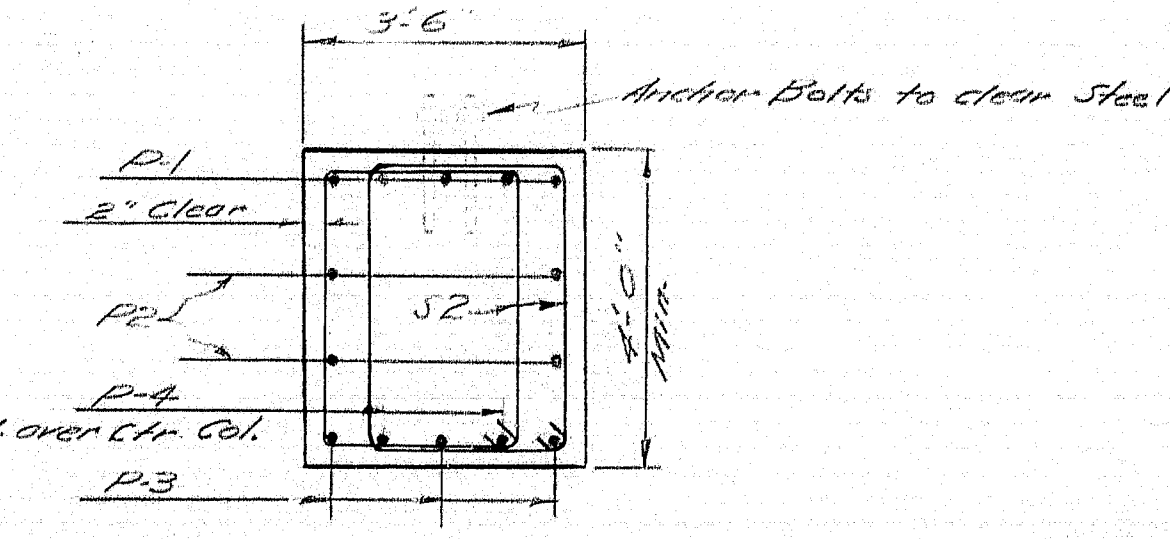
TABLE

LANE	PIER	COLUMN	ELEV. A	ELEV. B	* APPROXIMATE C BAR LEGE ELEVATION *	* LENGTH *	NUMBER OF SI BARS *
SOUTHBOUND	1	I	276.11	278.0	275.0	20'-0"	18
		II	276.11	278.0	275.0	20'-0"	18
		III	276.11	278.0	275.0	20'-0"	18
	2	I	276.46	281.0	278.0	17'-6"	16
		II	276.46	281.0	278.0	17'-6"	16
		III	276.46	281.0	278.0	17'-6"	16
NORTHBOUND	1	I	275.68	279.5	276.5	19'-0"	16
		II	275.68	279.5	276.5	17'-6"	15
		III	275.68	279.5	276.5	16'-6"	14
	2	I	276.23	280.0	278.0	17'-6"	16
		II	276.23	279.0	276.0	15'-0"	14
		III	276.23	281.0	278.0	17'-6"	16

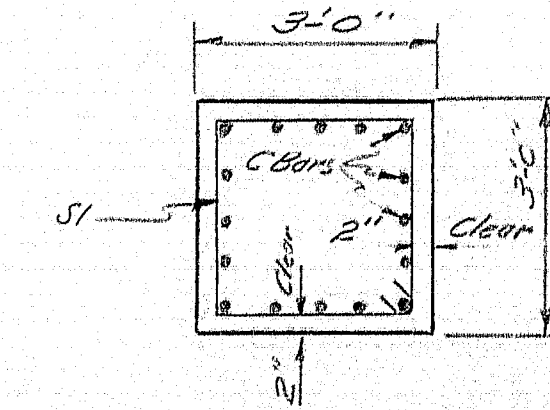
* Because the ledge elevations may vary from those elevations listed above, C Bars and SI Bars are not to be ordered until the ledge is exposed by structural excavation and a definite ledge elevation is established. Then 1' of concrete, 6 Bar lengths and the number of SI Bars are to be ordered by the Engineer to fit the situation.



END ELEVATION



SECTION B-B



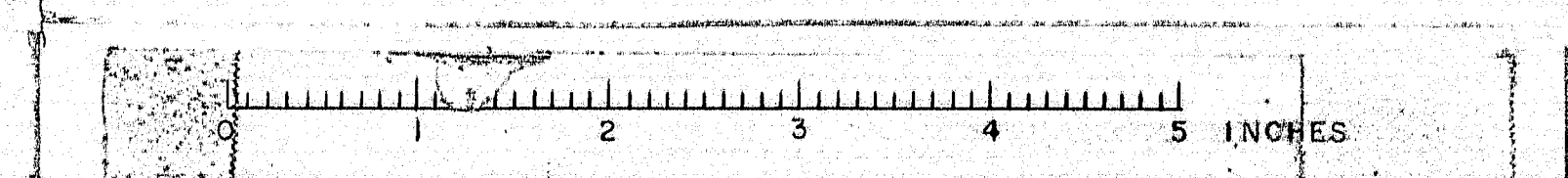
SECTION A-A

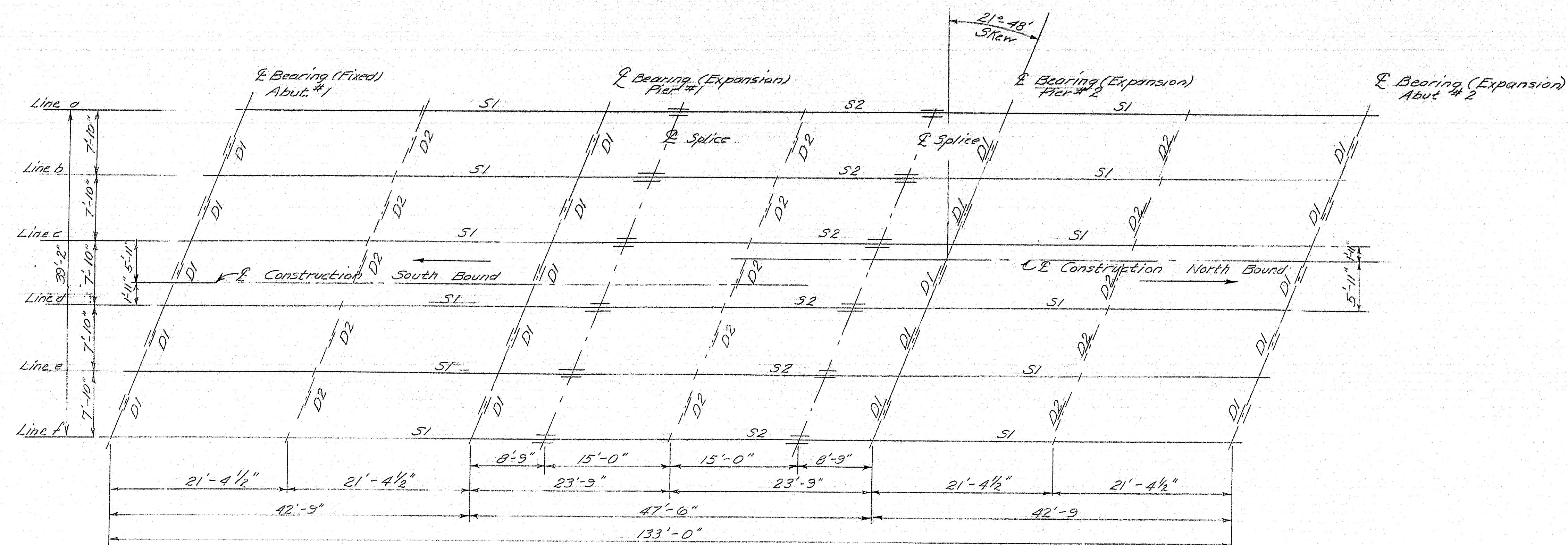
DESIGN: KHS
 CHECK: BLS

STATE HIGHWAY COMMISSION
 BRIDGE DIVISION
INTERSTATE 95
 OVER
NORTH MAIN STREET
 IN THE TOWN OF
PITTSFIELD
SOMERSET COUNTY
 PIERS, NORTHBOUND AND SOUTHBOUND

SHEET 14 OF 19 AUGUSTA, MAINE MARCH 1963

86-195

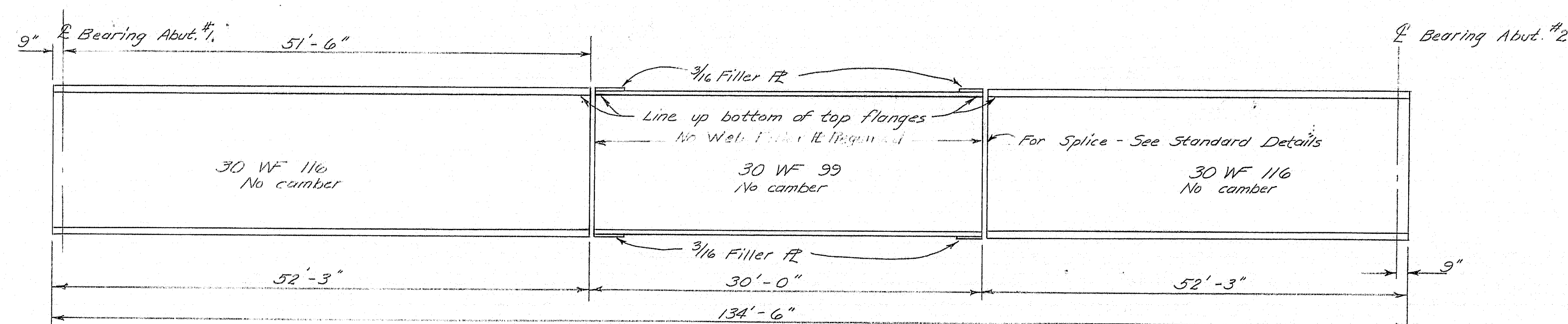




ERECTION DIAGRAM

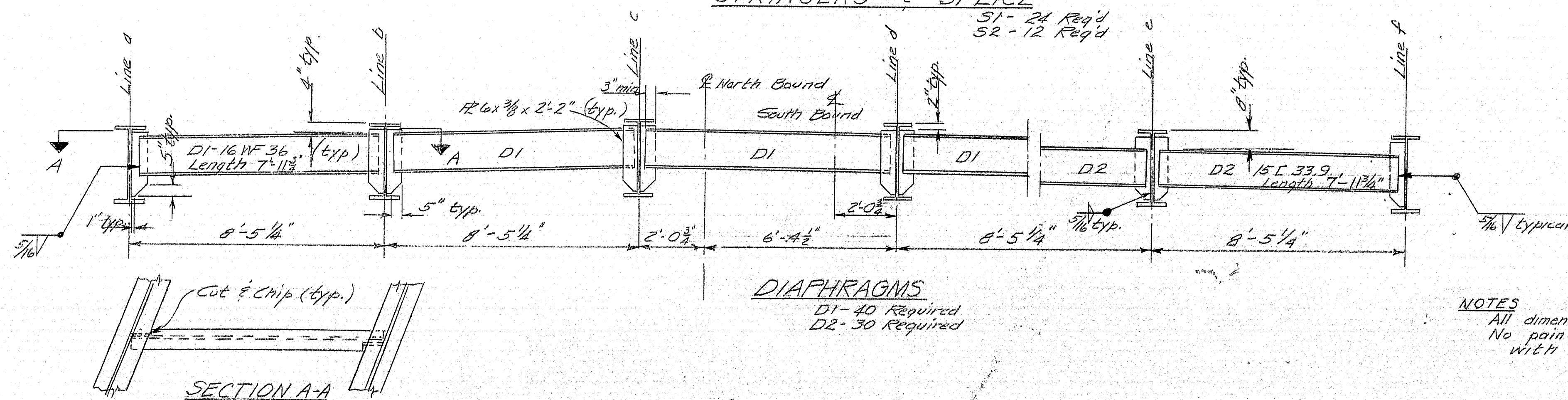
Note: For stringer grades see sheet #16. These grades are to be used to determine the levels of the stringer ends at the splices.

For Pedestals - See Standard Details
Bearing Pedestals
12 FPA Required @ Abut #1
36 EPA Required @ Pier #1, Pier #2 & Abut #2



STRINGERS & SPLICE

51'-24 Reg'd
52'-12 Reg'd

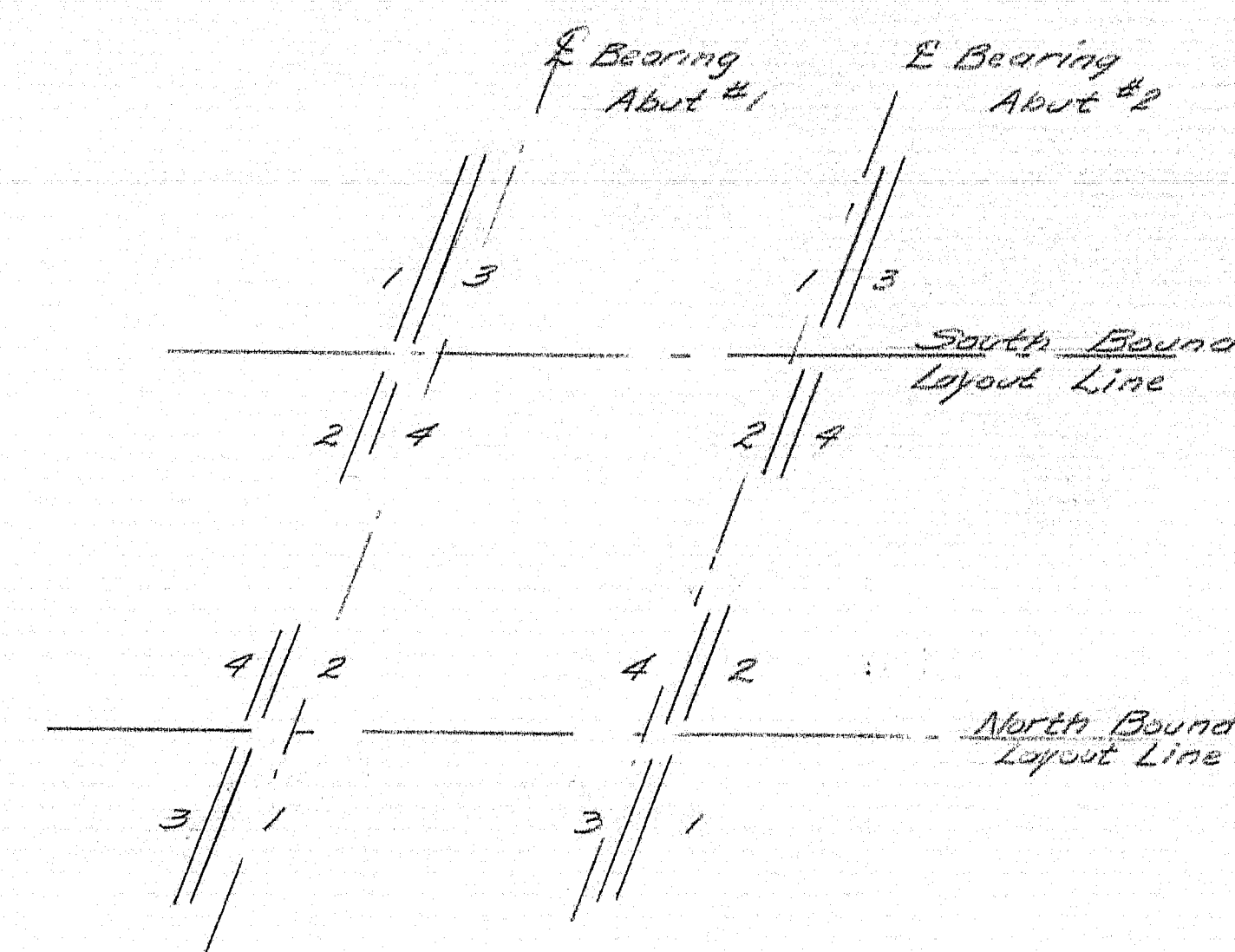


DIAPHRAGMS

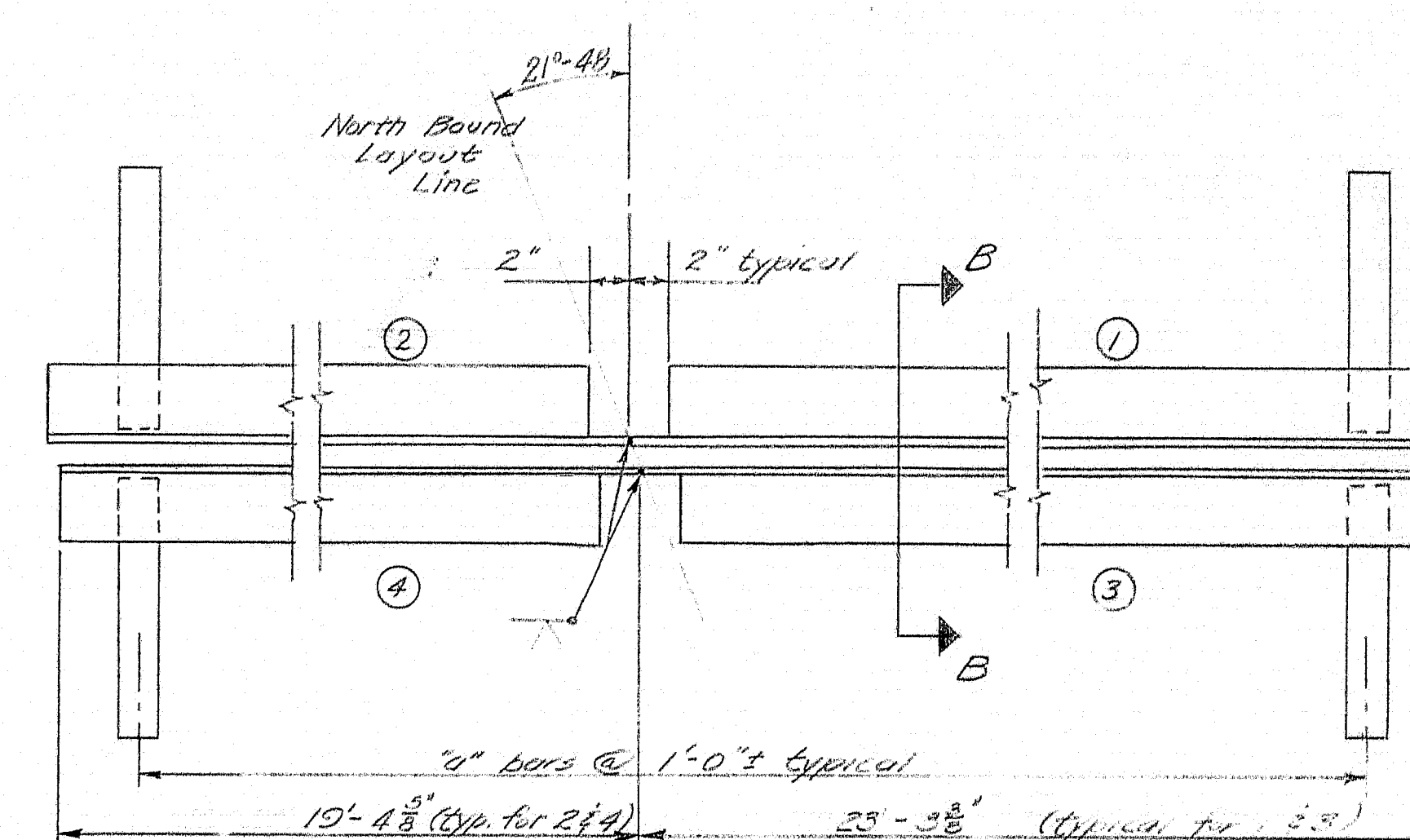
D1-40 Required
D2-30 Required

NOTES

All dimensions are horizontal.
No point where concrete is in contact with steel.



LOCATION OF ARMORED JOINTS

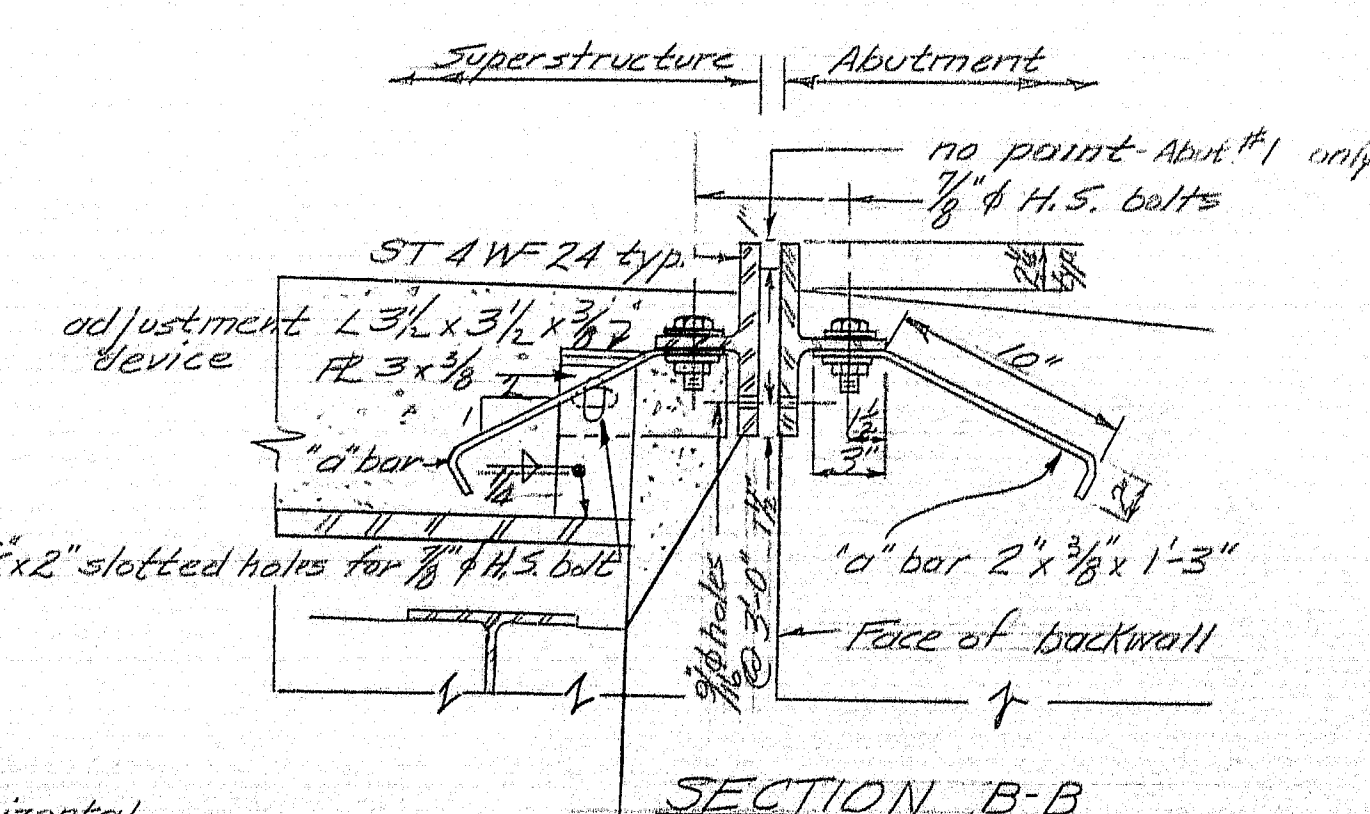


ARMORED JOINT

See "Location of Armored Joints" for South Bound Lane

SPECIFICATIONS

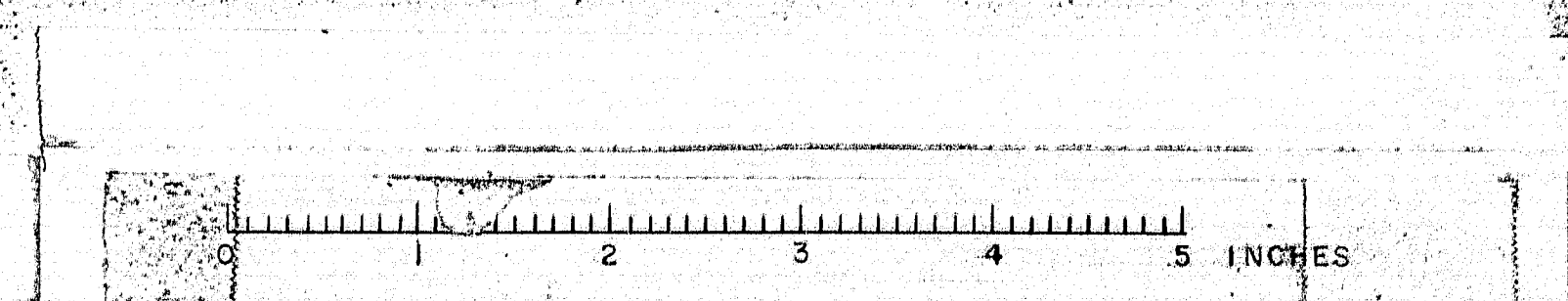
Foundation & Erection: State of Maine, Standard Specifications, Highways & Bridges, Revision of Jan. 1956, and Supplements. Design and Detail: AASHTO Standard Specifications of 1961, and revisions. Materials: Stringers & splice plates shall conform to A.S.T.M. designation A-36. Other members shall conform to A.S.T.M. designation A7 or A36.

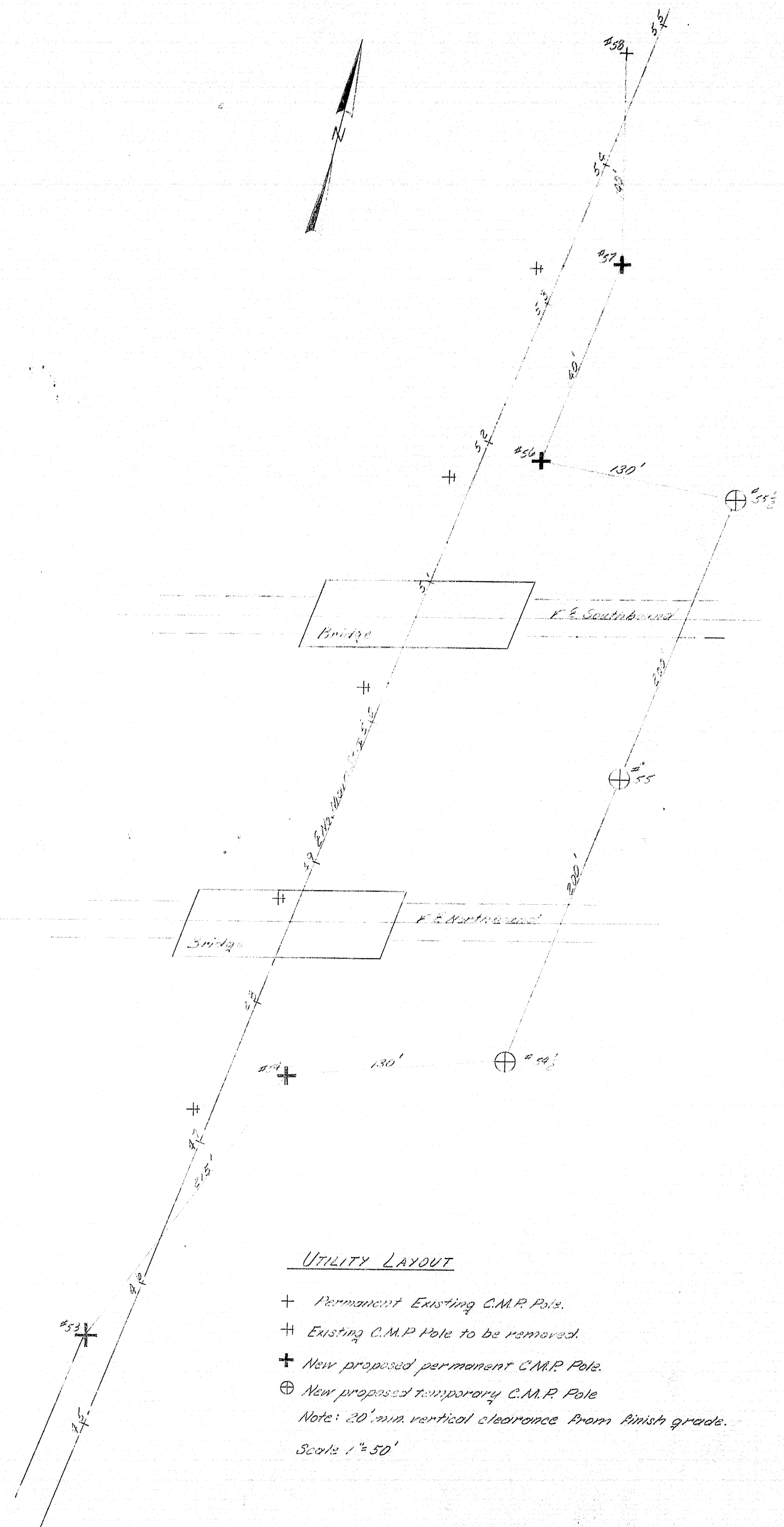


SECTION B-B

DESIGN - T.H.K.
DETAIL - PAUL
CHECK - 12/66
BRIDGE NO. 1
STATE HIGHWAY COMMISSION
BRIDGE DIVISION
INTERSTATE 95
OVER
NORTH MAIN STREET
IN THE TOWN OF
PITTSFIELD
SOMERSET COUNTY
STRUCTURAL STEEL
SHEET 15 OF 19 AUGUSTA, MAINE MARCH 1963

86-196

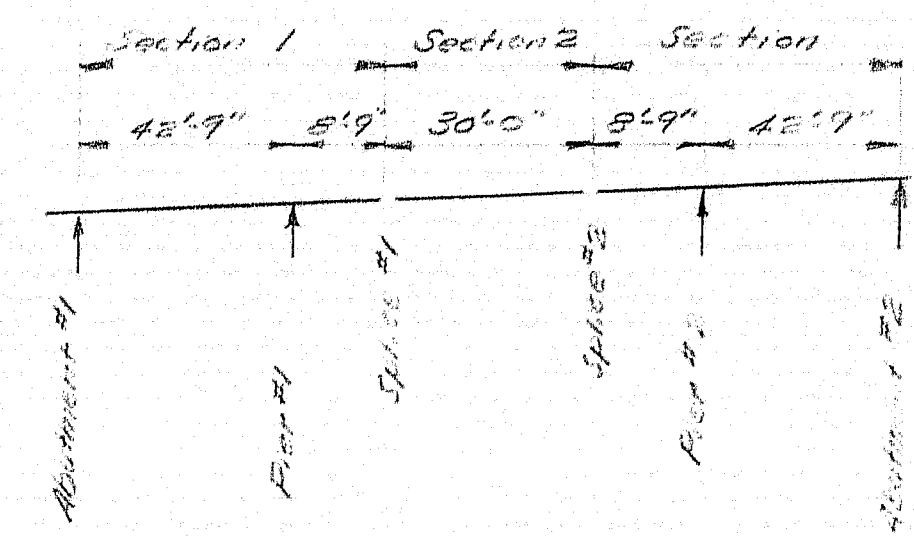




STRINGER SLOPES EXPRESSED IN PERCENT OF GRADE

Stringer	Northbound		
	1	2	3
a	1.3	1.1	0.91
b	1.3	1.1	0.91
c	1.3	1.1	0.94
d	1.3	1.2	0.94
e	1.3	1.2	0.94
f	1.4	1.1	0.98

Stringer	Southbound		
	1	2	3
a	0.58	0.60	0.42
b	0.58	0.63	0.44
c	0.91	0.61	0.41
d	0.94	0.61	0.41
e	0.94	0.73	0.49
f	0.94	0.73	0.49



GRADE DIAGRAM
Typical Both NB & SB

BRIDGE QUANTITIES		
ITEM	UNIT	QUANTITY
Structural Earth Excavation - Piers	CY	350
*Bituminous Concrete Surface Course - Type A	Tons	135
Portland Cement Concrete - Abuts & Retaining Walls	C.Y.	400
Portland Cement Concrete - Piers	C.Y.	195
Portland Cement Concrete - Rdwy & SW Slabs on Steel Br.	C.Y.	320
Portland Cement	bbls	1380
Structural Steel, Fabricated & Delivered	L.S.	L.S.
Structural Steel, Erection	L.S.	L.S.
Structural Steel, Field Painting	L.S.	L.S.
Reinforcing Steel, Delivered	Lbs	134,700
Reinforcing Steel, Placing	Lbs	134,700
Steel H-Beam, Piles, 12"/ft	Lin. Ft.	1358
Aluminum Rail - Alternate "A"	Lin. Ft.	606
Steel Rail - Alternate "B"	Lin. Ft.	606
*Membrane Waterproofing	S.Y.	1230
Epoxy Resin Surface Sealant	S.Y.	180
Granite Bridge Curb	Lin. Ft.	624

Note: The estimated weight of Structural Steel is 230,700 lbs, which includes drains.
*These items not a part of this contract.

UTILITY LAYOUT

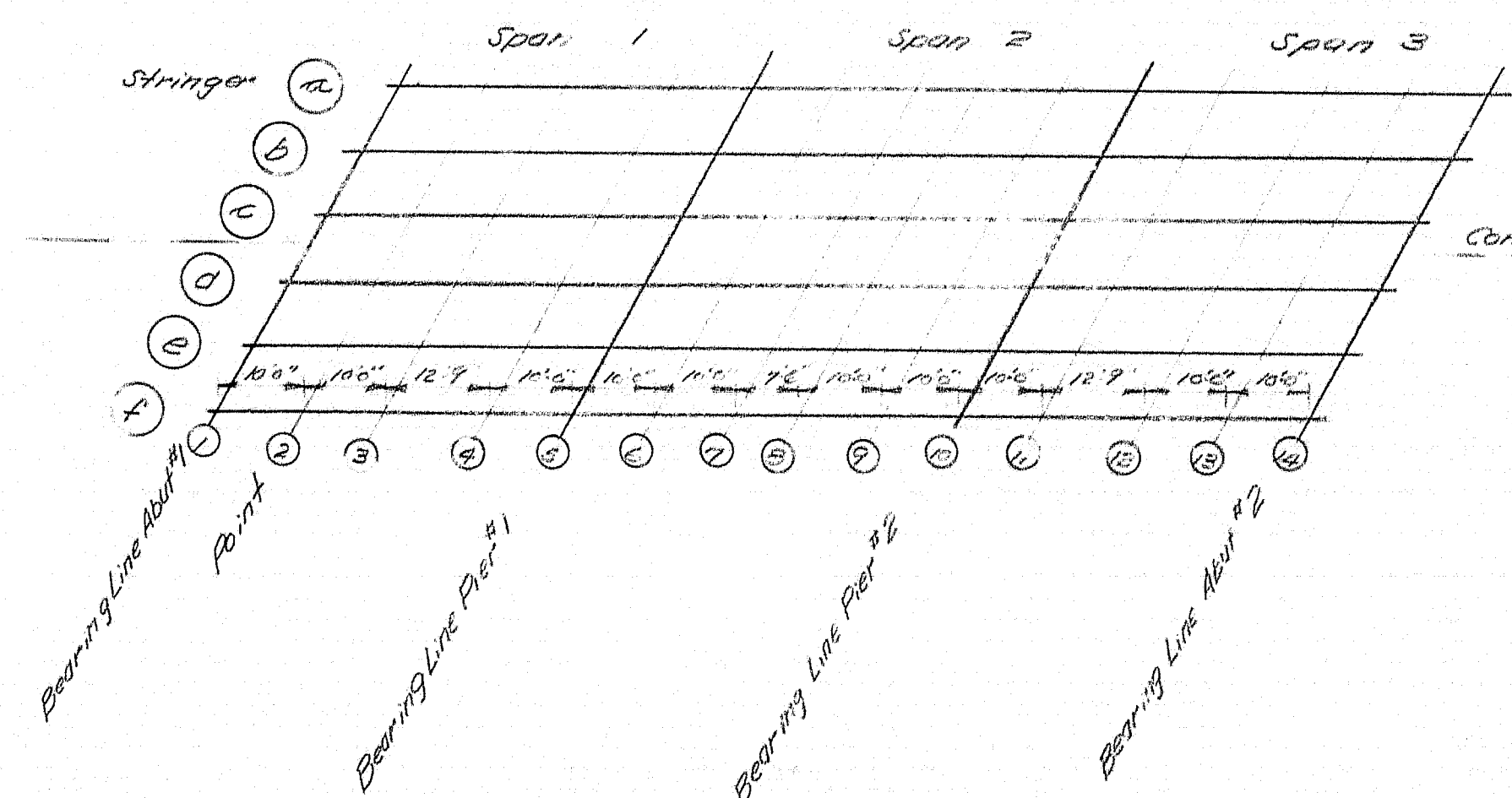
- + Permanent Existing C.M.P. Pole.
 - + Existing C.M.P. Pole to be removed.
 - + New proposed permanent C.M.P. Pole.
 - ⊕ New proposed temporary C.M.P. Pole
- Note: 20' min. vertical clearance from finish grade.
Scale: 1" = 50'

BLOCKING SCHEDULE

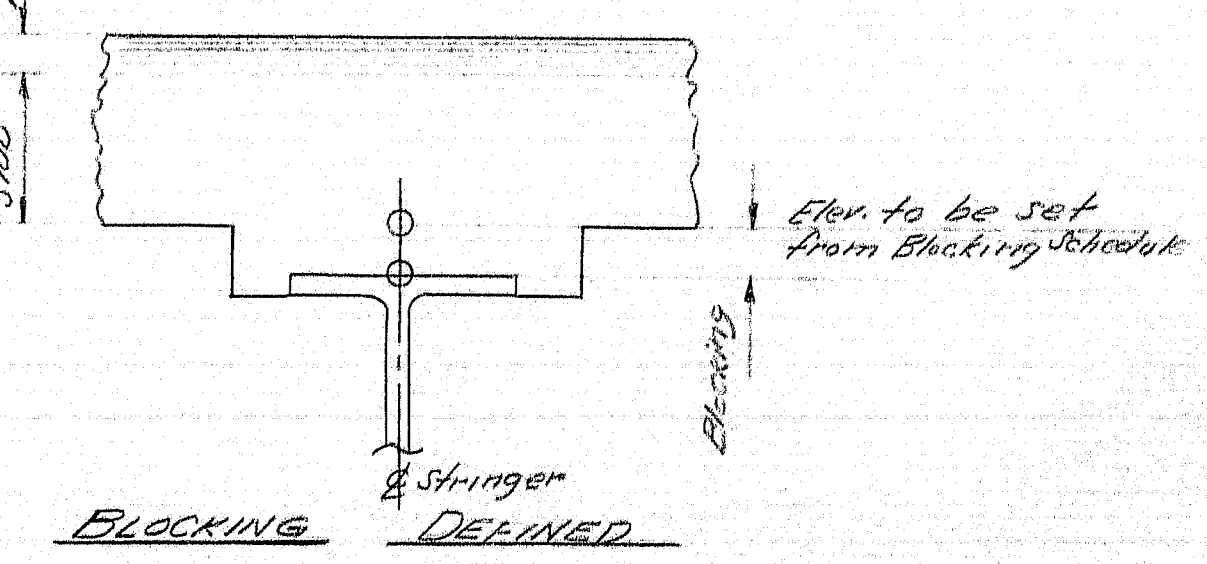
SOUTH BOUND		STRINGER					
SPAN	Point	a	b	c	d	e	f
1	1	302.76	302.86	302.96	303.00	302.83	302.67
	2	302.87	302.98	303.08	303.12	302.96	302.79
	3	302.97	303.08	303.19	303.23	303.06	302.89
	4	303.06	303.17	303.28	303.32	303.16	302.99
	5	303.13	303.24	303.35	303.40	303.23	303.07
2	6	303.20	303.32	303.43	303.47	303.31	303.15
	7	303.28	303.40	303.51	303.56	303.40	303.24
	8	303.33	303.45	303.56	303.61	303.45	303.29
	9	303.38	303.50	303.61	303.66	303.51	303.35
	10	303.43	303.55	303.67	303.72	303.57	303.42
3	11	303.49	303.62	303.74	303.79	303.64	303.48
	12	303.57	303.69	303.81	303.87	303.72	303.57
	13	303.60	303.72	303.85	303.91	303.76	303.61
	14	303.61	303.74	303.87	303.92	303.78	303.63

NORTH BOUND		STRINGER					
SPAN	Point	a	b	c	d	e	f
1	1	302.35	302.44	302.53	302.42	302.24	302.06
	2	302.51	302.60	302.69	302.58	302.40	302.22
	3	302.65	302.74	302.83	302.72	302.54	302.36
	4	302.79	302.89	302.98	302.87	302.70	302.52
	5	302.90	303.00	303.10	302.99	302.81	302.64
2	6	303.02	303.12	303.22	303.11	302.94	302.76
	7	303.14	303.24	303.34	303.24	303.06	302.89
	8	303.23	303.33	303.43	303.32	303.15	302.98
	9	303.32	303.43	303.53	303.43	303.26	303.08
	10	303.42	303.53	303.63	303.53	303.36	303.19
3	11	303.53	303.64	303.74	303.64	303.47	303.31
	12	303.66	303.77	303.88	303.78	303.62	303.45
	13	303.75	303.86	303.96	303.87	303.70	303.54
	14	303.81	303.92	304.03	303.93	303.77	303.61

Note: In order to compensate for dead load deflections and normal irregularities in the stringers, produced in the rolling process, set the elevations in the table above at the points indicated below before any of the slab forms are started.



BLOCKING DIAGRAM

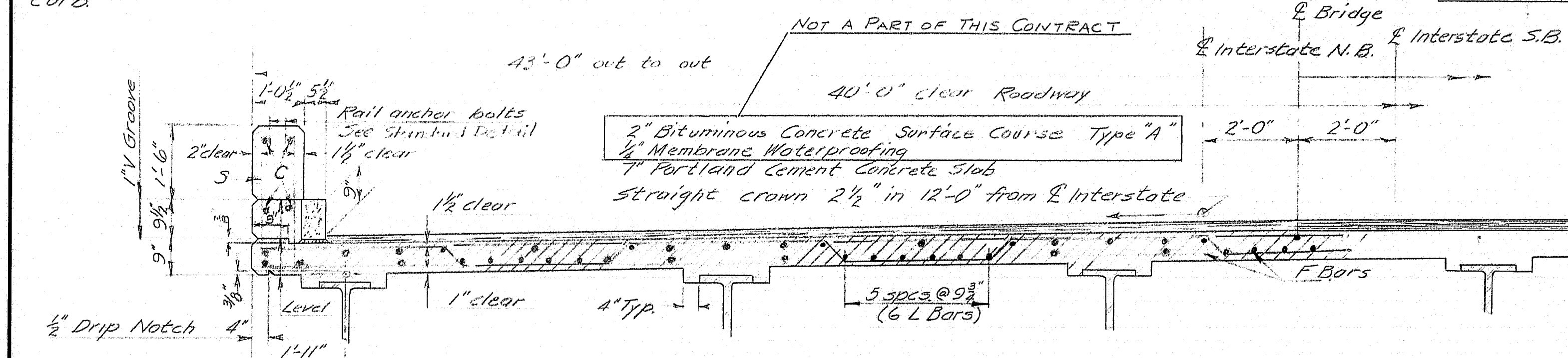
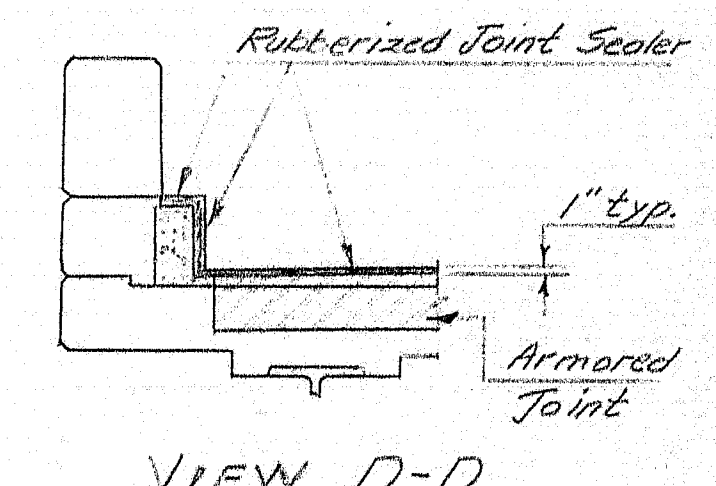
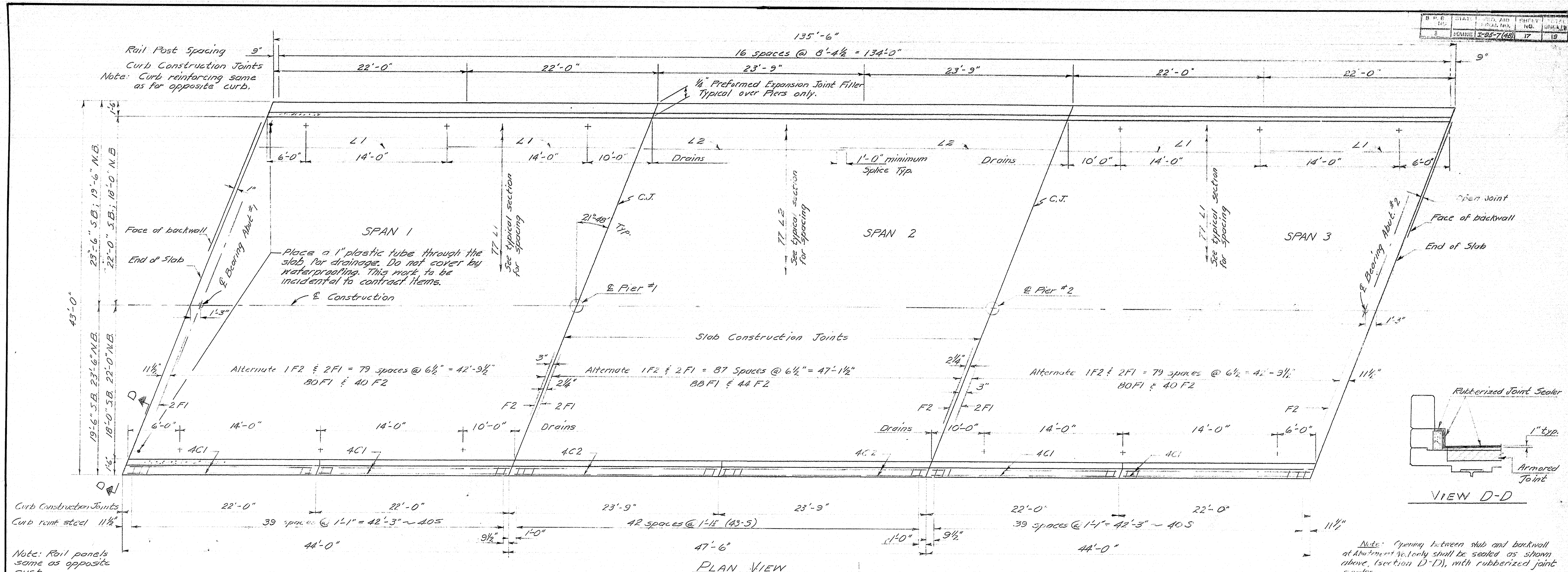


DESIGN - T.H.K. & A.L.L.
DETAIL - ALYON
CHECK - [Signature]

STATE HIGHWAY COMMISSION
BRIDGE DIVISION

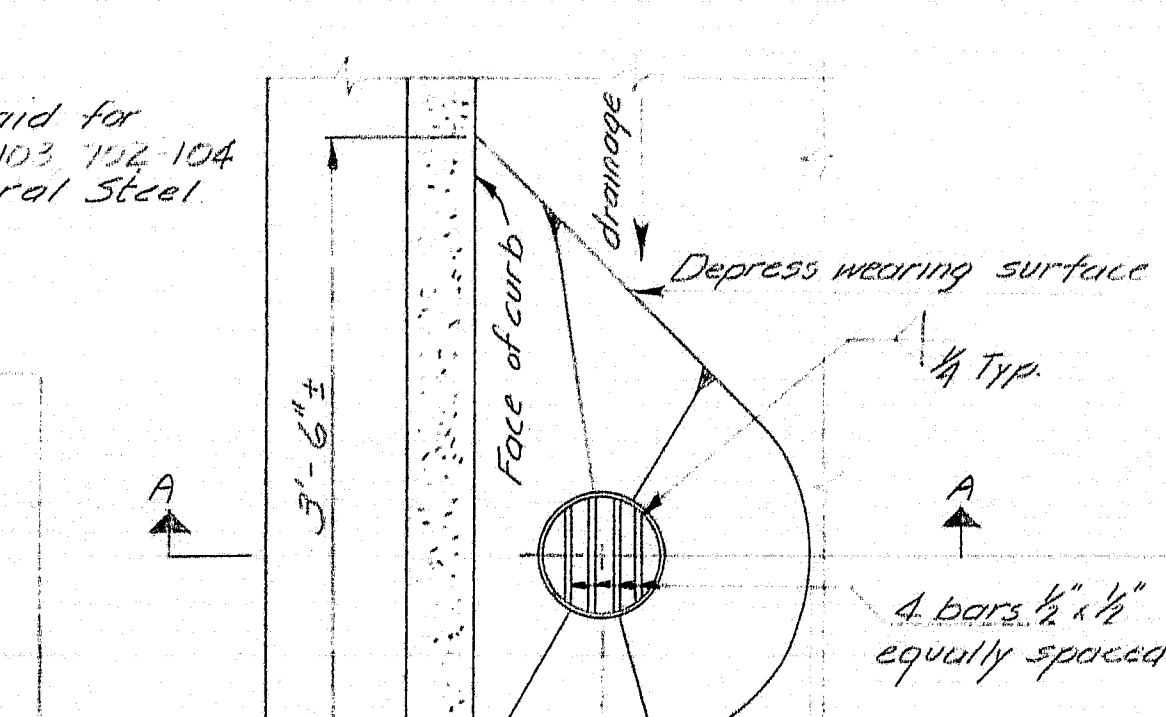
INTERSTATE 95
OVER
NORTH MAIN STREET
IN THE TOWN OF
PITTSFIELD
SOMERSET COUNTY

BLOCKING SCHEDULE
SHEET 16 OF 19 AUGUSTA, MAINE MARCH 1963



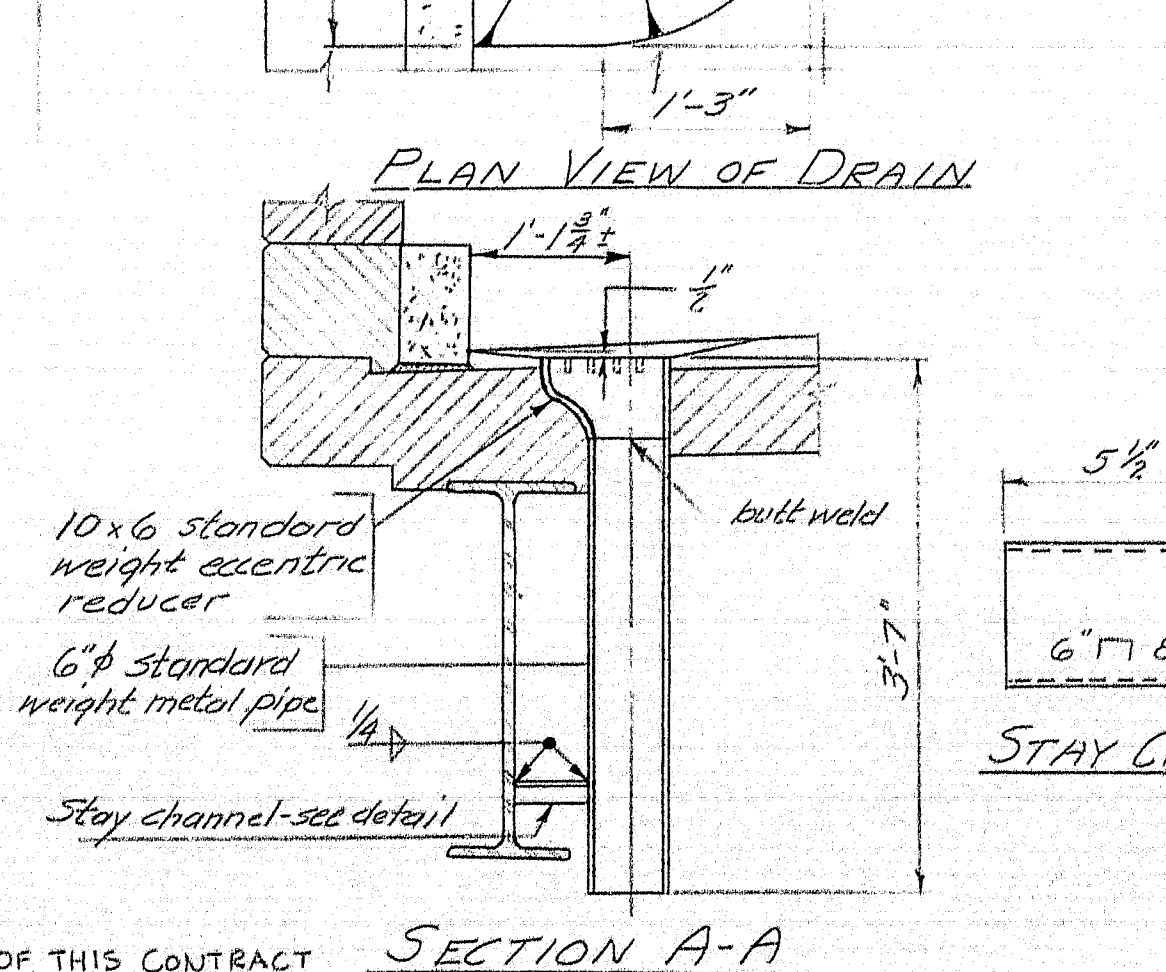
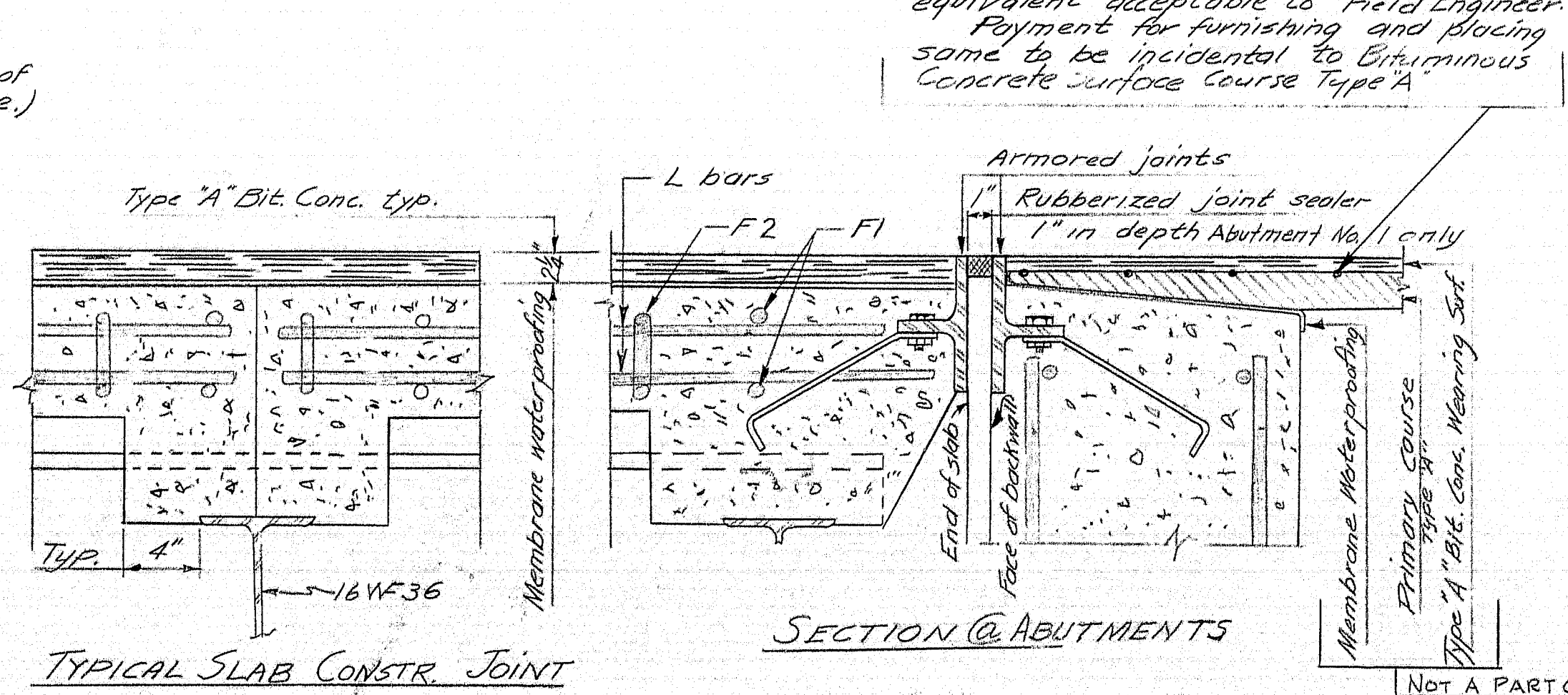
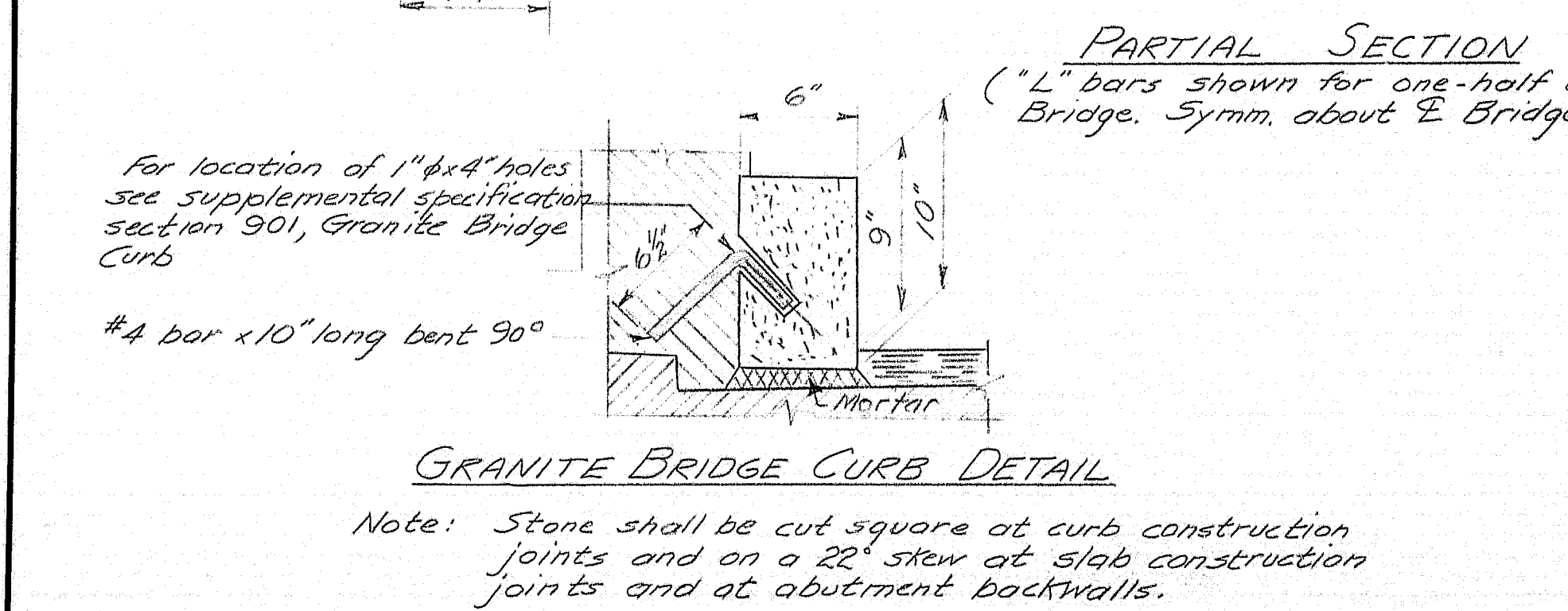
Note: Drains shall be paid for under Items 702-103, 104, 104 & 702-105, Structural Steel

Note: 36" minimum width 4x4 - 10 ga. galvanized welded wire mesh or equivalent acceptable to Field Engineer. Payment for furnishing and placing same to be incidental to Bituminous Concrete Surface Course Type "A"



GENERAL NOTES

Contract Membrane waterproofing and the bituminous concrete item are to be done by others. Concrete Placement: The placing of concrete shall be regulated so that the middle of a span is placed first. Concrete for curbs shall not be placed until slab concrete has been in place 7 days. During the 7 days forms may be constructed, but only hand equipment will be allowed in the slab. Chamber all exposed concrete edges 1/2". Rail - For rail detail see Standard Sheet 60-102. Joints - Break bond at all construction joints with a coat of heavy asphalt paint.

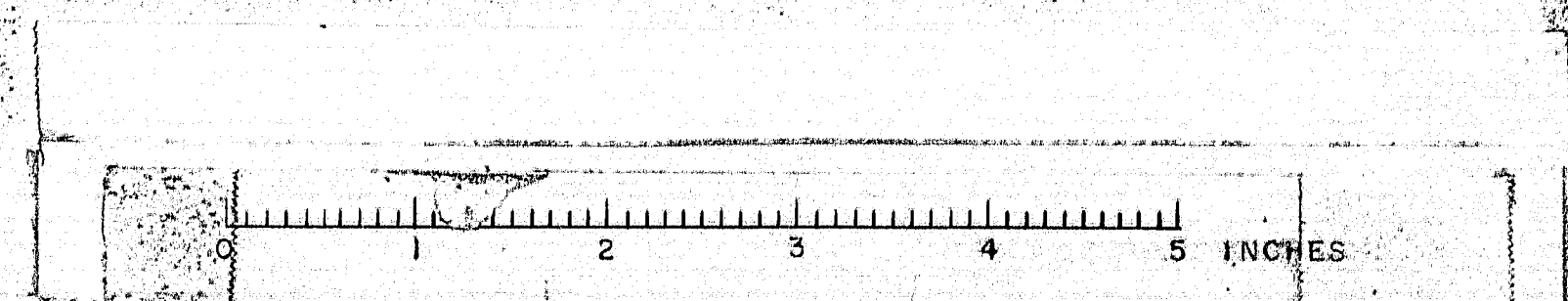


DESIGN: T.H.K., DET. H.F.
CHECK: B.G.M.

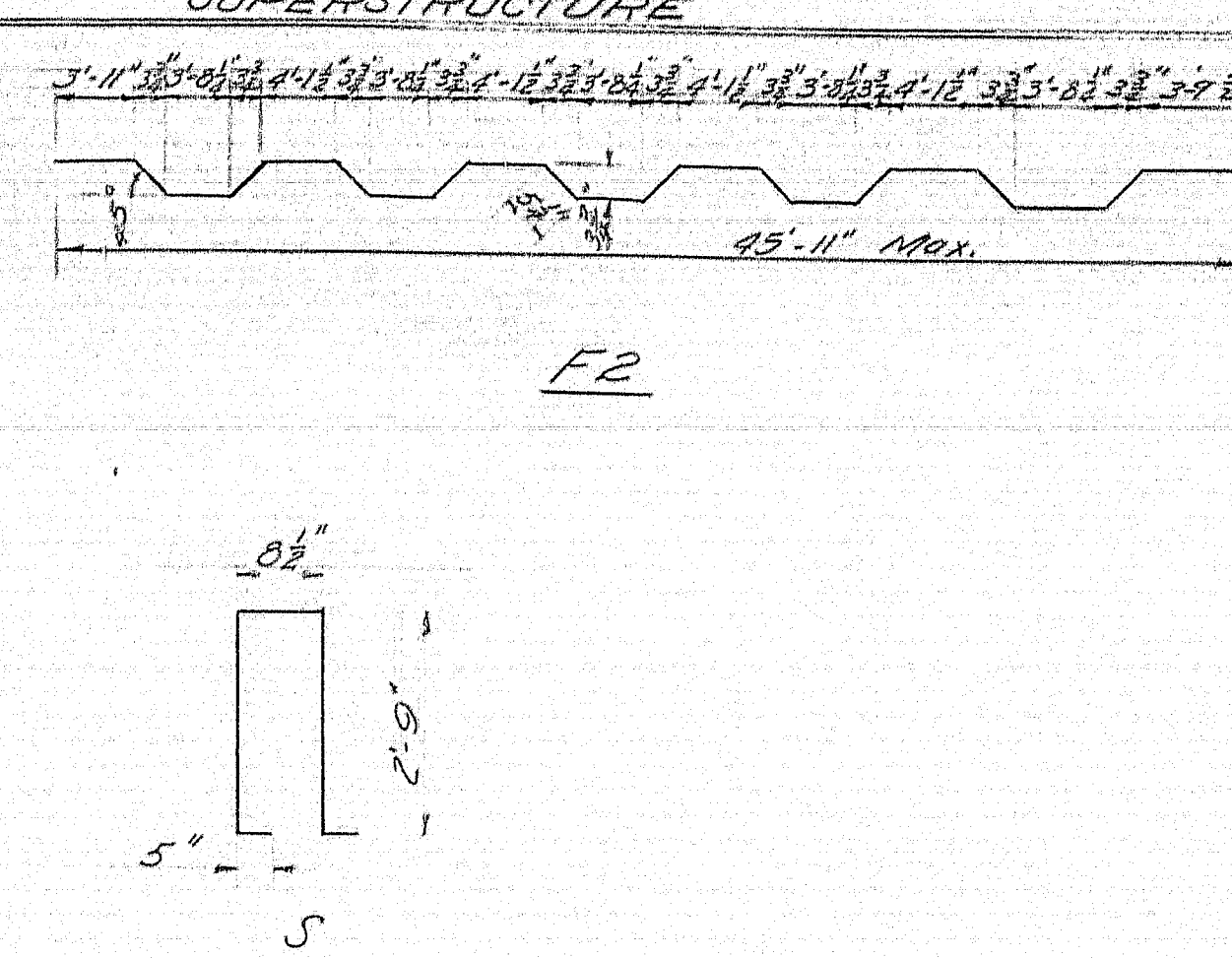
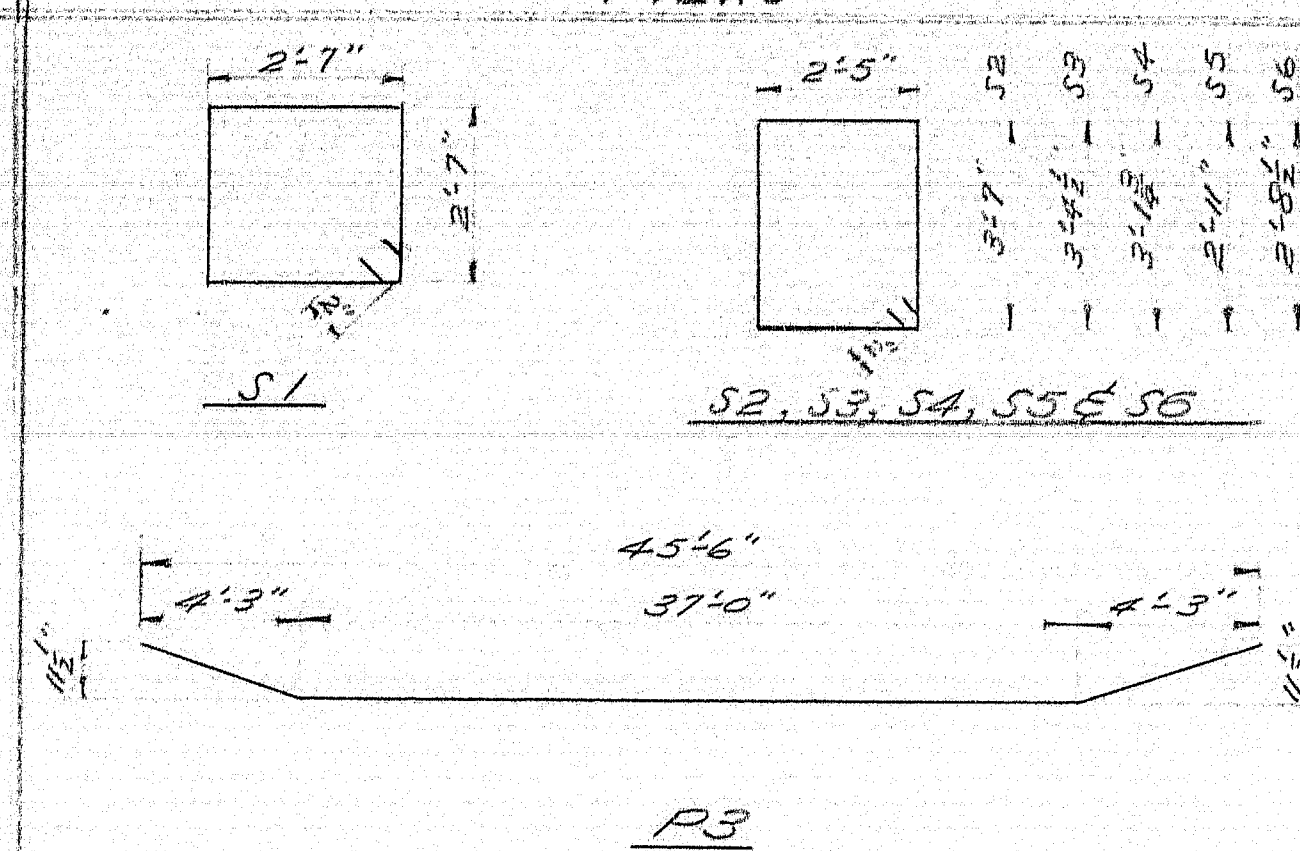
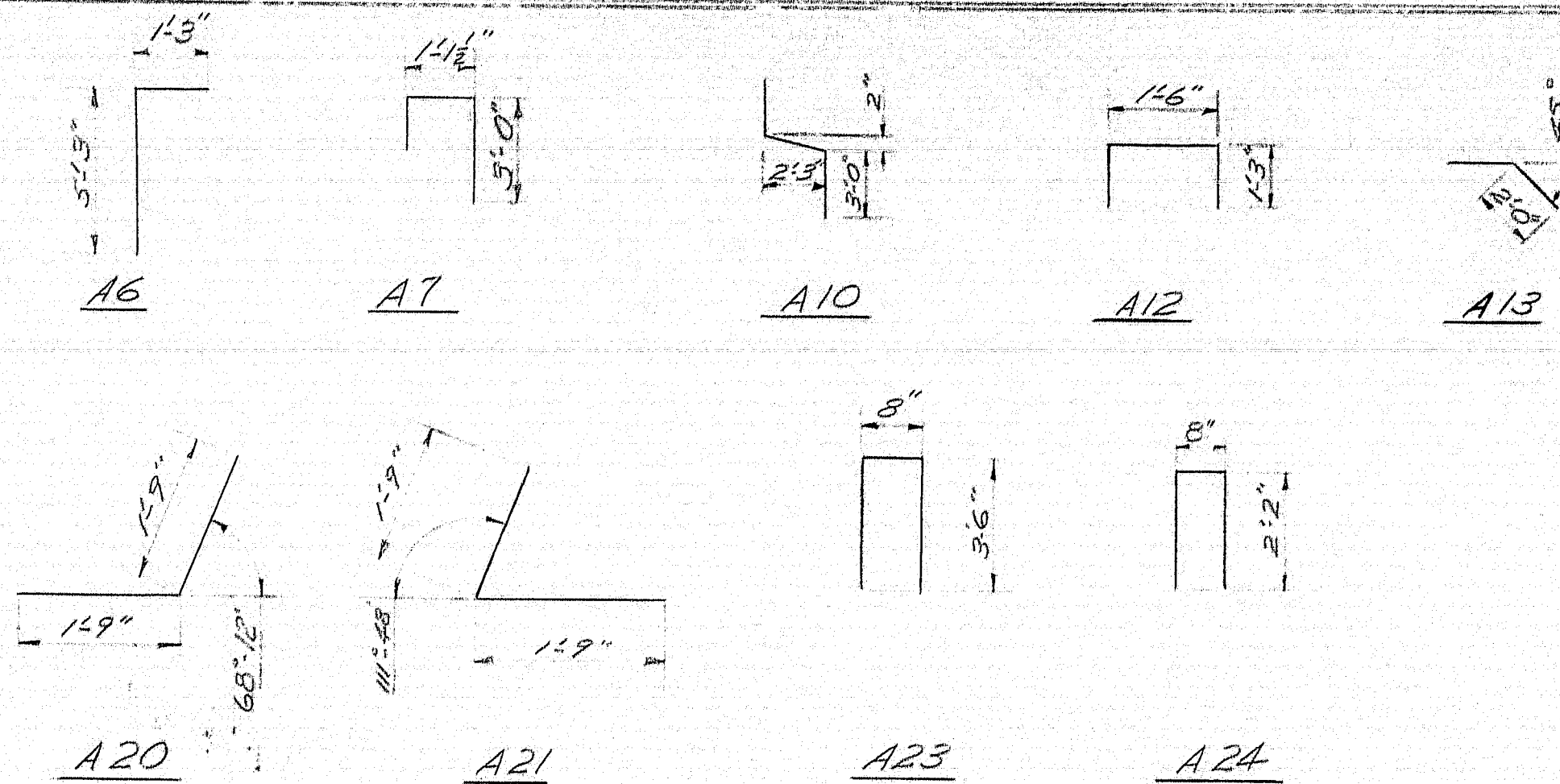
STATE HIGHWAY COMMISSION
BRIDGE DIVISION

INTERSTATE 95
OVER
NORTH MAIN STREET
IN THE TOWN OF
PITTSFIELD
SOMERSET COUNTY
SUPERSTRUCTURE

SHEET 17 OF 19 AUGUSTA, MAINE MARCH 1963



SUPERSTRUCTURE



BENT BARS				
Bar	Size	Number	Length	Location
A6	#5	116	6'6"	Bridge Seats
A7	5	116	5'9"	Backwalk
A10	5	116	7'0"	Bridge Seat
A12	5	12	4'0"	Bearing Areas
A13	5	112	3'6"	Approach Stab
A20	6	36	3'6"	South Wing
A21	6	36	3'6"	North Wing
A23	4	80	7'6"	Wings
A24	#4	24	5'0"	End Posts

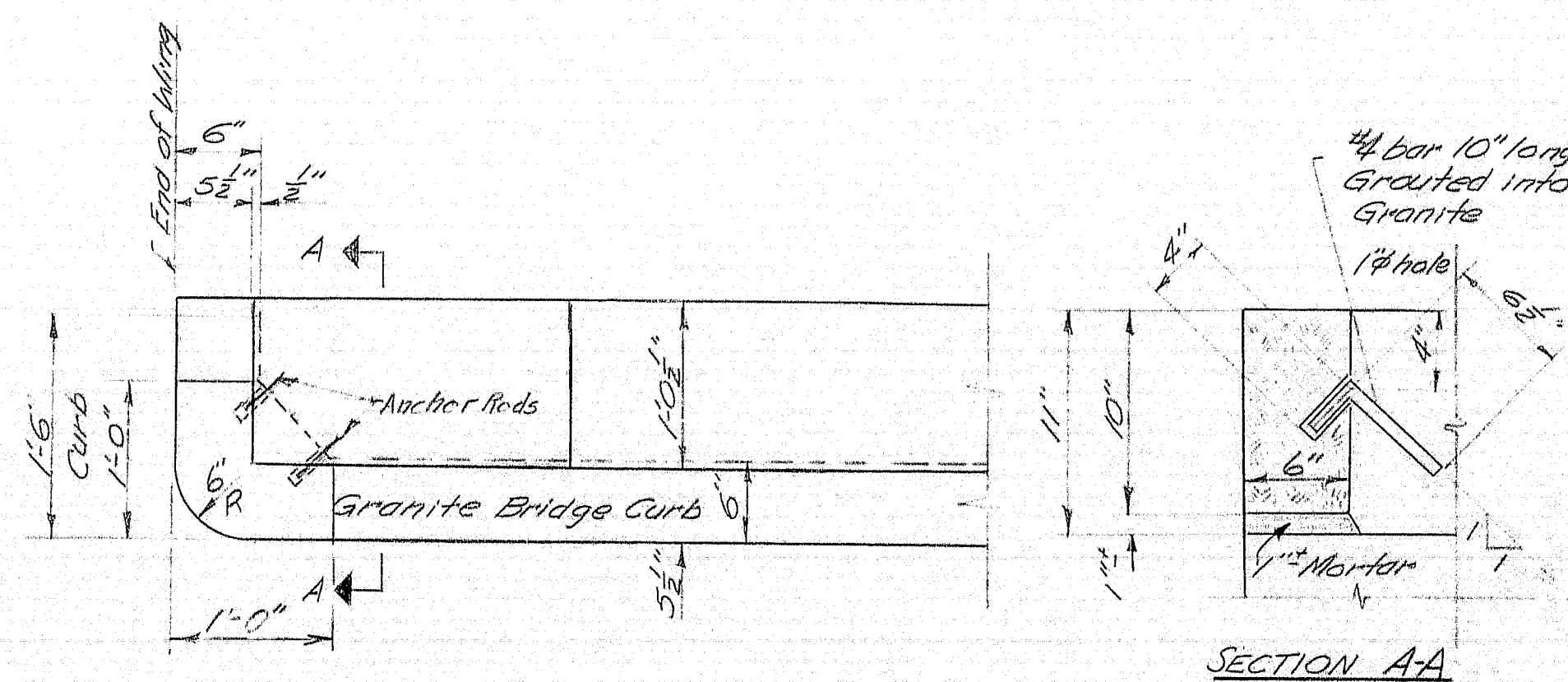
STRAIGHT BARS				
Bar	Size	Number	length	Location
A1	# 6	112	24'-9"	Footings
A2	6	336	6'-0"	Footings
A3	6	64	13'-9"	Footings
A4	6	120	3'-0"	Footings
A5	5	372	3'-0"	Footings to Br. Seat
A8	4	48	25'-6"	Br. Seat & Bk. Wall
A9	4	48	2'-0"	Br. Seat & Bk. Wall
A11	5	72	2'-8"	Bearing Areas
A14	5	60	5'-2"	Wings
A15	5	160	3'-8"	
A16	6	80	3'-0"	
A17	6	80	5'-4"	
A18	4	48	9'-6"	
A19	6	72	9'-6"	
A22	4	32	9'-0"	Wings
A25	# 4	32	1'-8"	End Posts
A51	4	80	38'-6"	Approach Slabs
A52	6	624	14'-6"	Approach Slabs
A26	4	16	9'-6"	Wings

BENT BARS			
Bar	Size	Number	Length Location
S1	# 4	195	10'-8" All Pier Columns
S2	4	160	12'-4" Pier Caps
S3	4	16	11'-11"
S4	4	16	11'-6"
S5	4	16	11'-0"
S6	4	16	10'-7"
P3	# 10	12	45'-8" Pier Caps

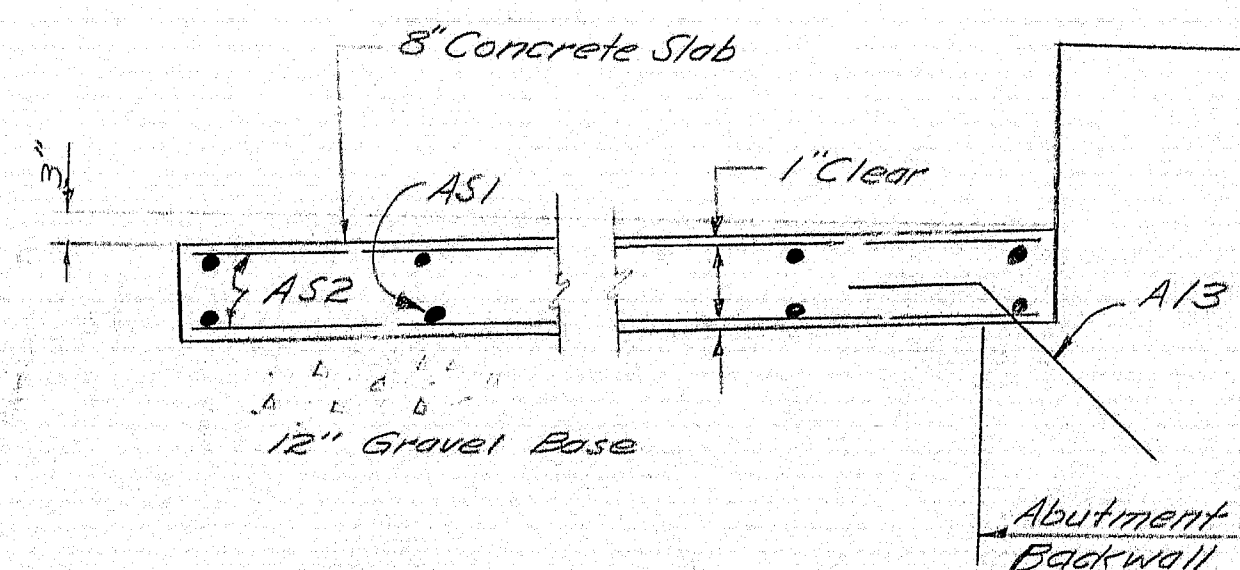
STRAIGHT BARS			
Bar	Size	Number	Length Location
P1	# 10	20	45'-6" Pier Caps
P2	6	16	45'-6" Pier Caps
P4	10	8	34'-0" Pier Caps
P5	6	240	4'-6" Footings
P6	8	192	4'-6" Dowels-Footings to
C1	8	16	16'-6" N.B. Pier 1
C2	8	96	17'-6" S.B. Pier 2 Col 1 & 3 N.B. Pier 1 & 2 - Col 1 & 2
C3	8	32	19'-0" N.B. Pier 1 & 2
C4	8	48	20'-0" S.B. Pier 1

BENT BARS				Location
Bar	Size	Number	Length	
F2	6	252	47'-2"	Adwy. 5106
S	4	212	7'-0"	Curve Pierapets

STRAIGHT BARS				Location
Bar	Size	Number	Length	
C1	4	64	21'-8"	Curve Pierapet
G2	4	32	25'-5"	Curve Pierapet
F1	6	496	45'-11"	Adwy. 5106
L1	6	308	22'-4"	Adwy. 5106
L2	6	154	24'-1"	Adwy. 5106



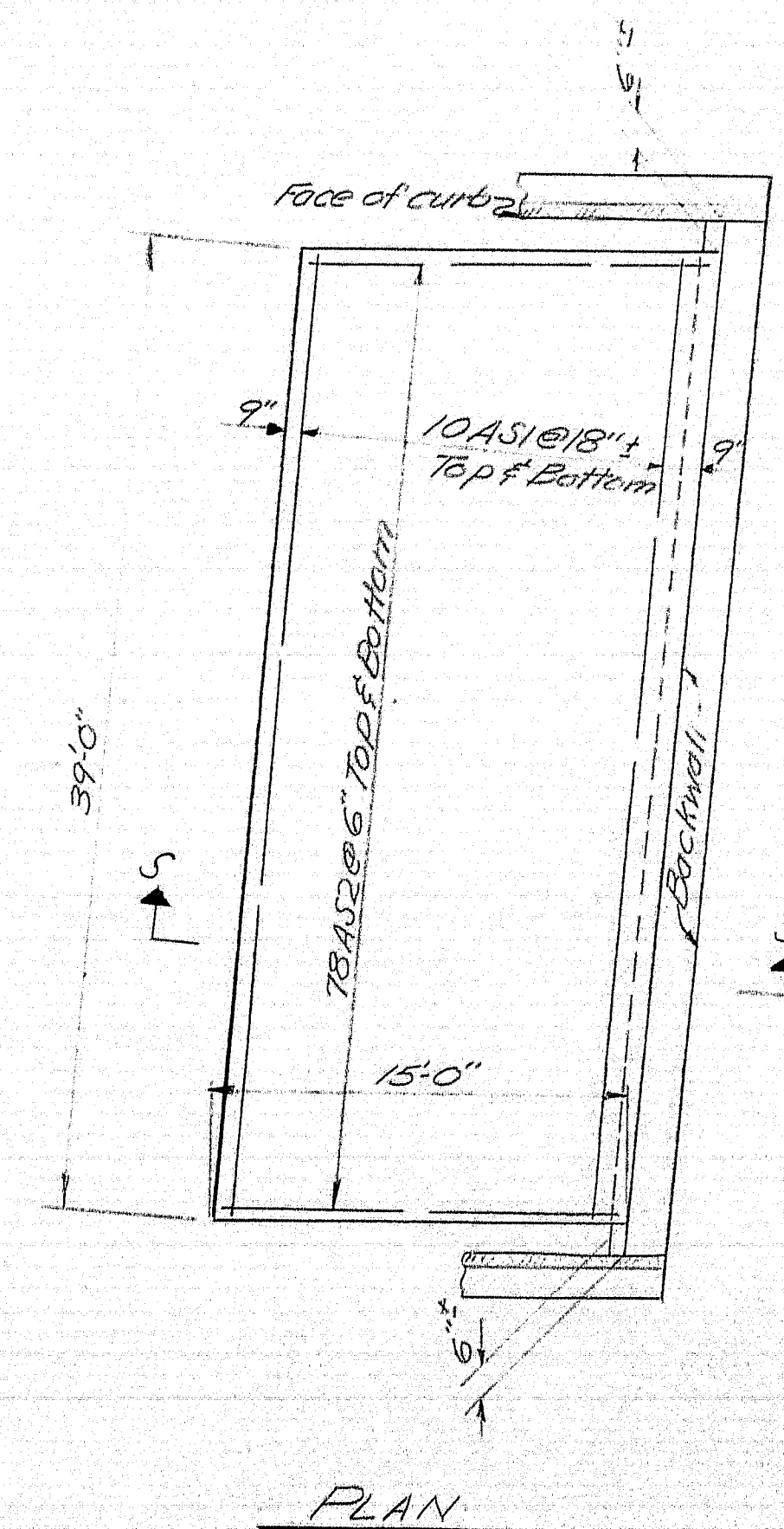
GRANITE BRIDGE CURB DETAIL
AT ABUTMENT WINGS



SECTION S-S

Payment for Concrete in approach slabs to be made under Item 701.33, Portland Cement Concrete Abuts. & Ret. Walls.

APPROACH SLAB DETAILS



PLAN

-Note-

- 1) Reinforcing steel to be of Intermediate Grade
- 2) Dimensions are to ϕ of Bars

DESIGN - T.H.K.
DETAIL - ALYON
CHECK - *Blake*

STATE HIGHWAY COMMISSION
BRIDGE DIVISION

INTERSTATE 95

OVER

NORTH MAIN STREET

IN THE TOWN OF

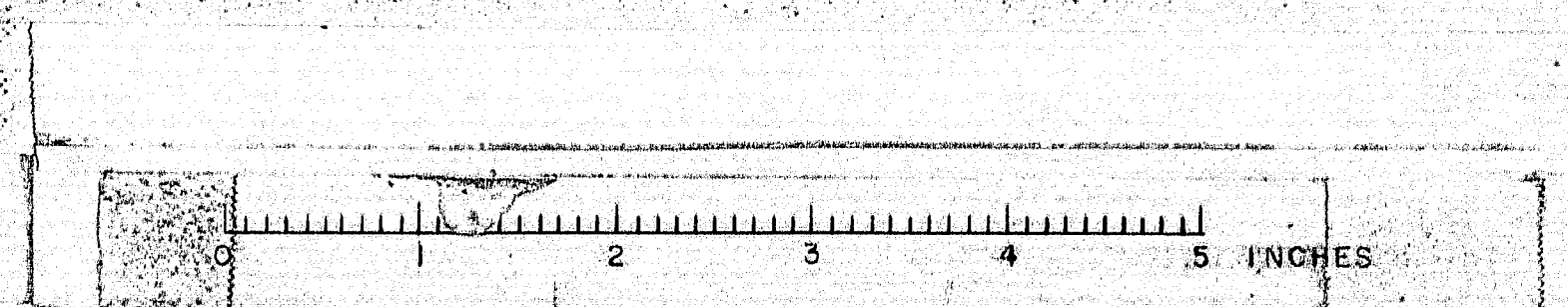
PITTSFIELD

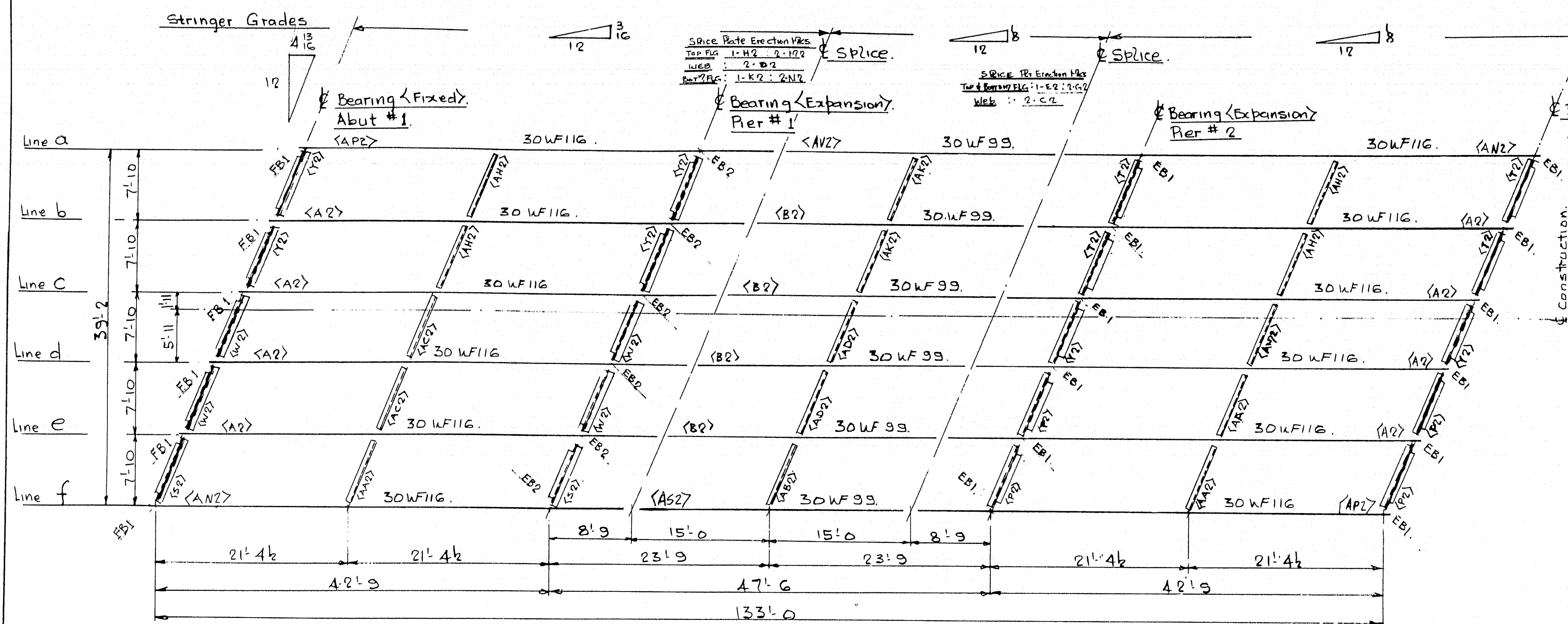
SOMERSET COUNTY

REINFORCING STEEL - GRANITE BRIDGE CURB
APPROACH SLAB DETAILS

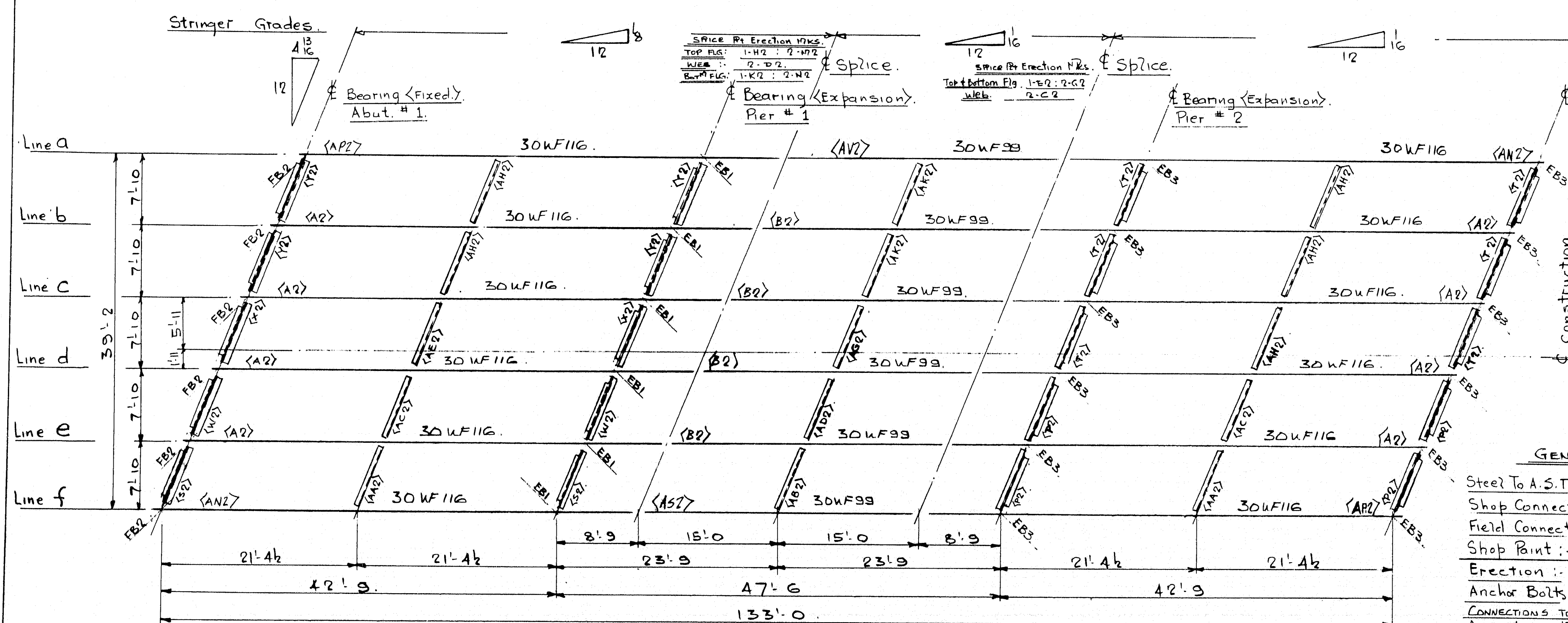
SHEET 19 OF 19 AUGUSTA, MAINE MARCH 1963

86-200



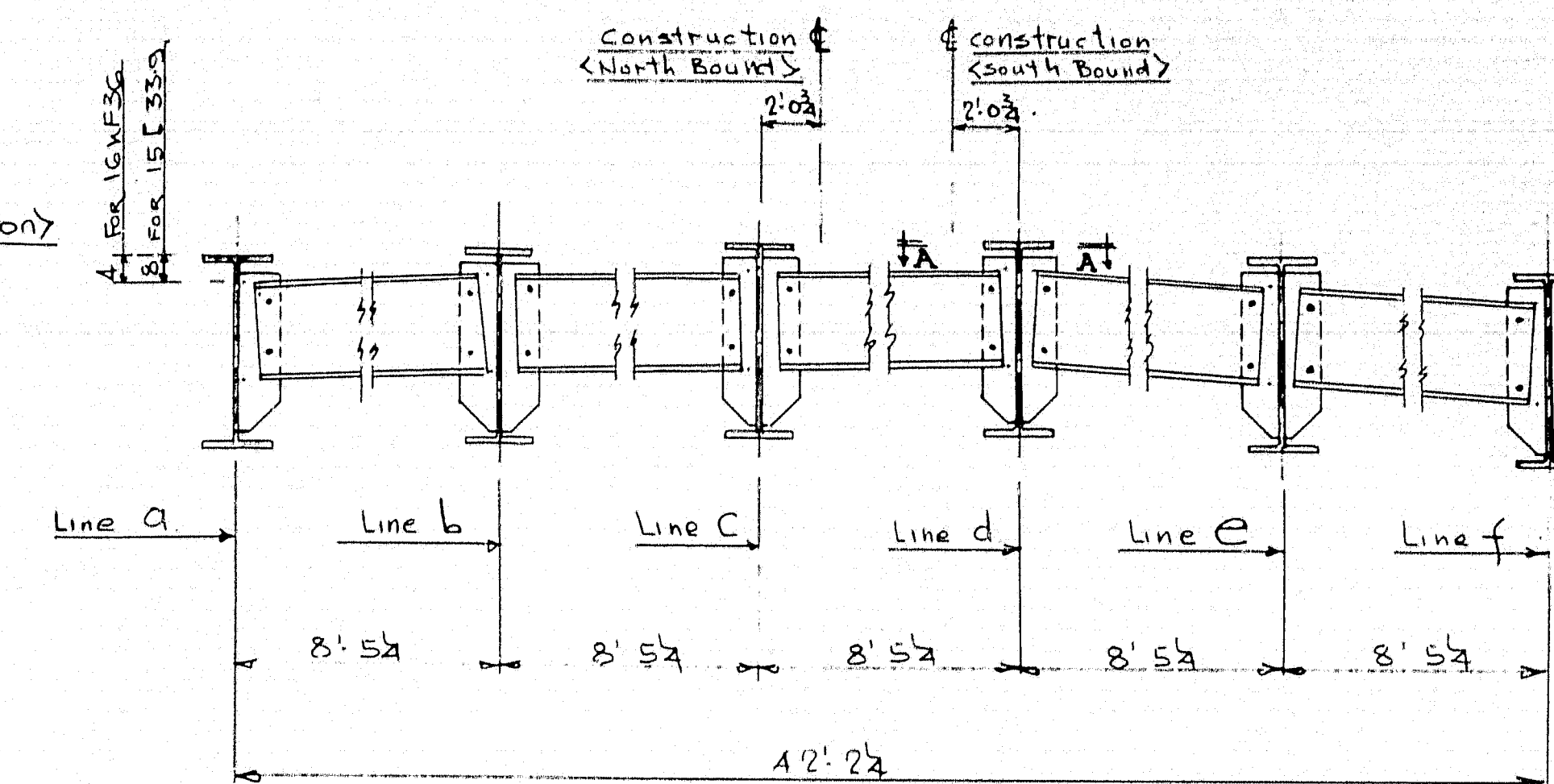


ERECTION DIAGRAM (North Bound)

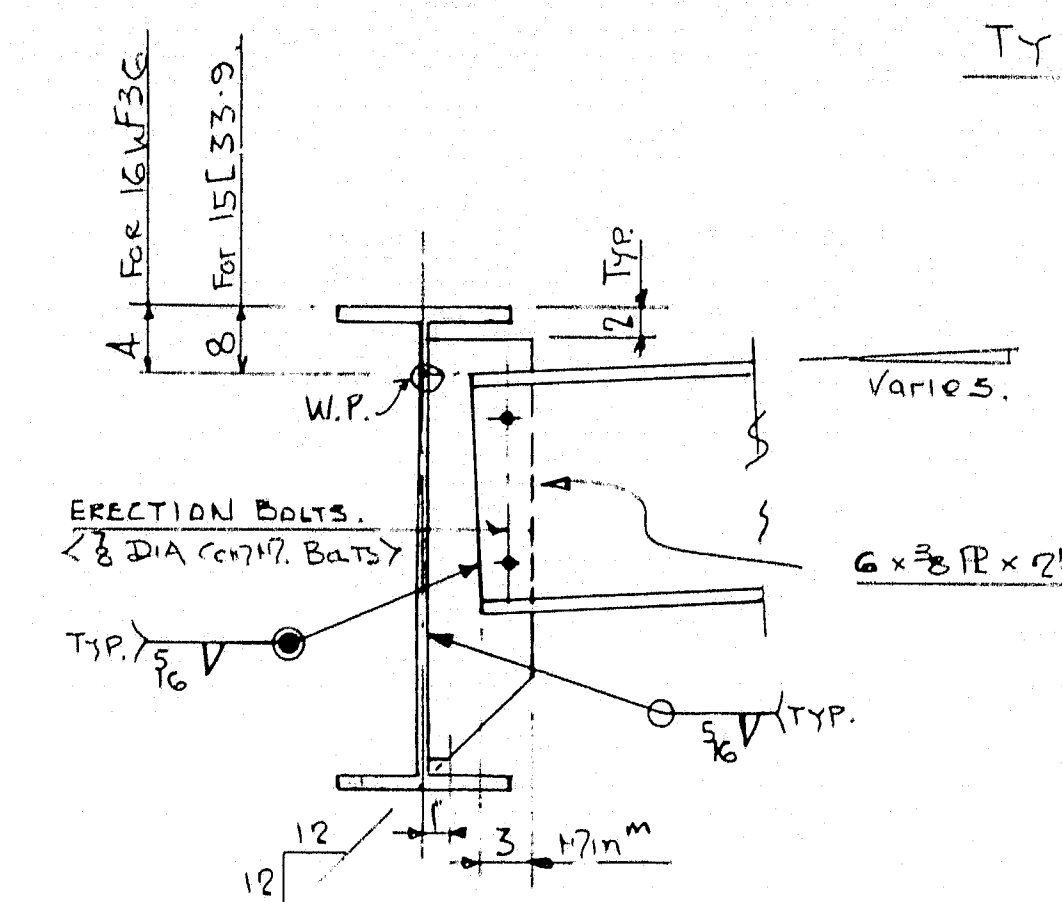


ERECTION DIAGRAM (South Bound)

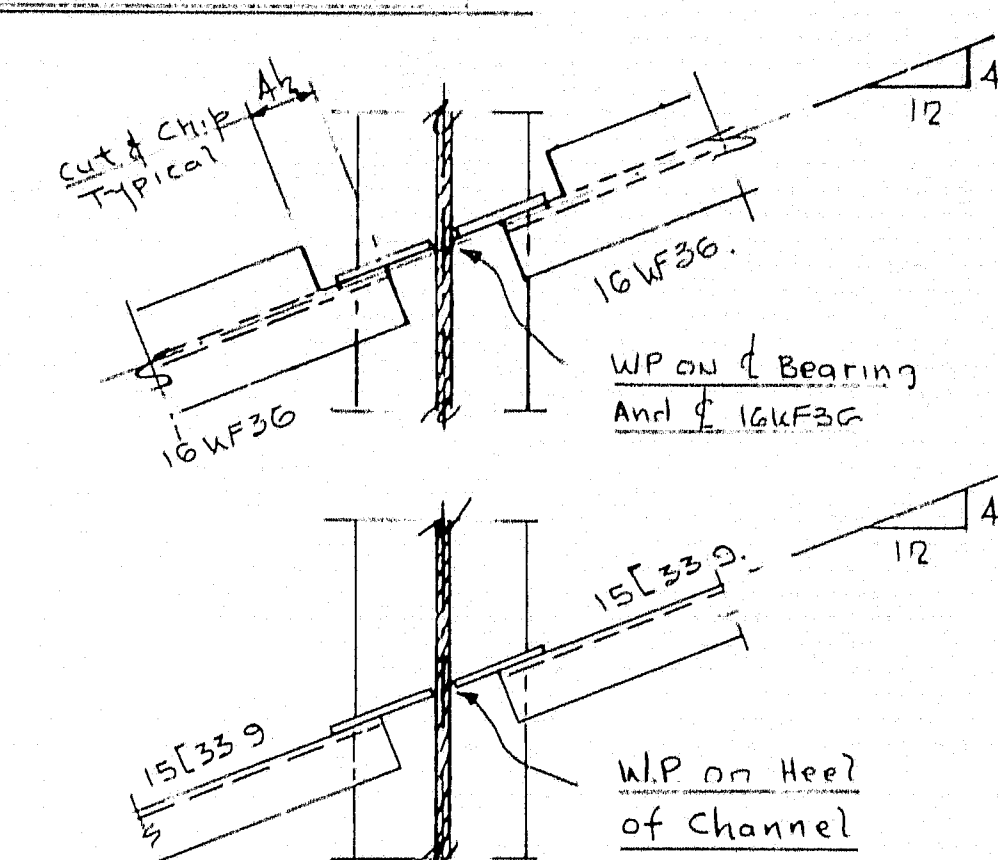
DIAPHRAGMS AT BEARINGS 16WF36.
INTERMEDIATE DIAPHRAGMS 15I33.9.
DIAPHRAGM SPACING DIMENSIONS ARE
TO 16WF36 & BACK OF 15I33.9.



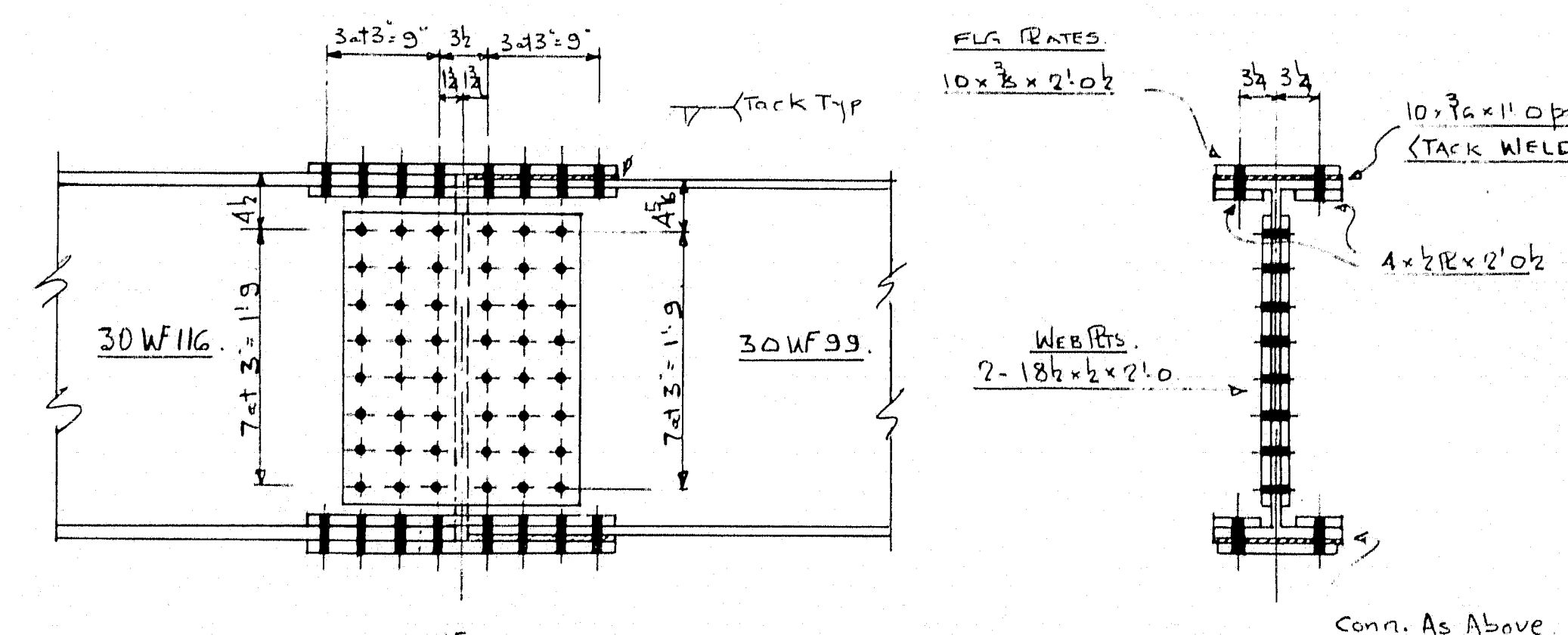
TYPICAL CROSS SECTION



TYPICAL DIAPHRAGM CONNECTION



PLAN ON A-A



TYPICAL SPICE DETAIL

GENERAL NOTES

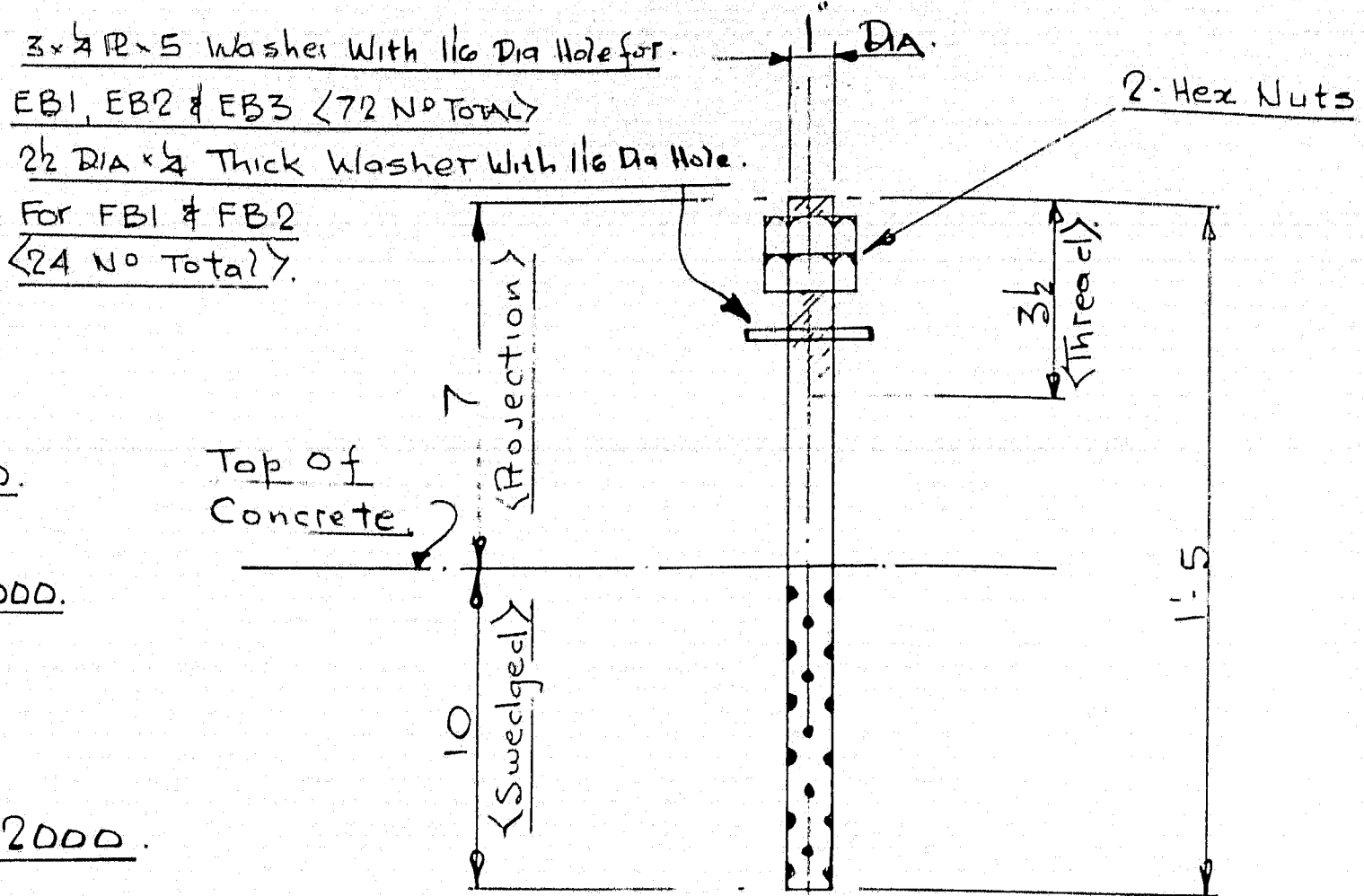
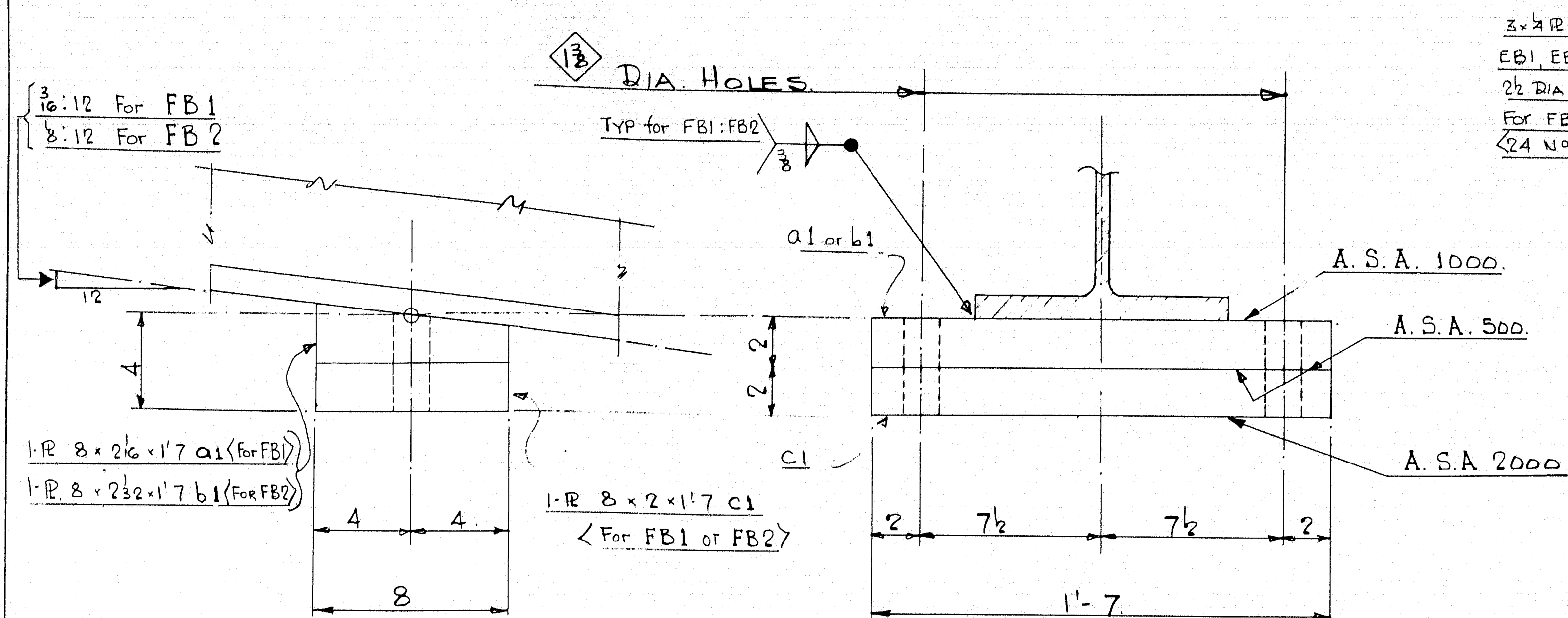
Steel To A.S.T.M. A36 (Latest Revision) See Specification.
Shop Connections: Welded.
Field Connections: Welded & H.T. Bolts.
Shop Paint: STD. RED PRIMER UN.
Erection: By Others.
Anchor Bolts: Supplied By A.I.W. Set By Others.
Connections To Be SUB-PUNCHED 1/4" AND REAMED
ASSEMBLED TO 1/4" OR DRILLED ASSEMBLED TO 1/4"

ERECTOR NOTE

Members To Be Erected So That
Marked End Is In Same Location
As Shown on This Drawing.

REVISIONS	DESCRIPTION	DATE
A	JOB: BRIDGE OVER NORTH MAIN STREET	
B	PITTSFIELD - MAINE	
C	ARCHT. STATE OF MAINE BRIDGE DEPT.	
D	CUSTOMER: REED & REED	
E	BATH - MAINE	
F	AUGUSTA IRON WORKS	
G	AUGUSTA, MAINE	
H		
J		

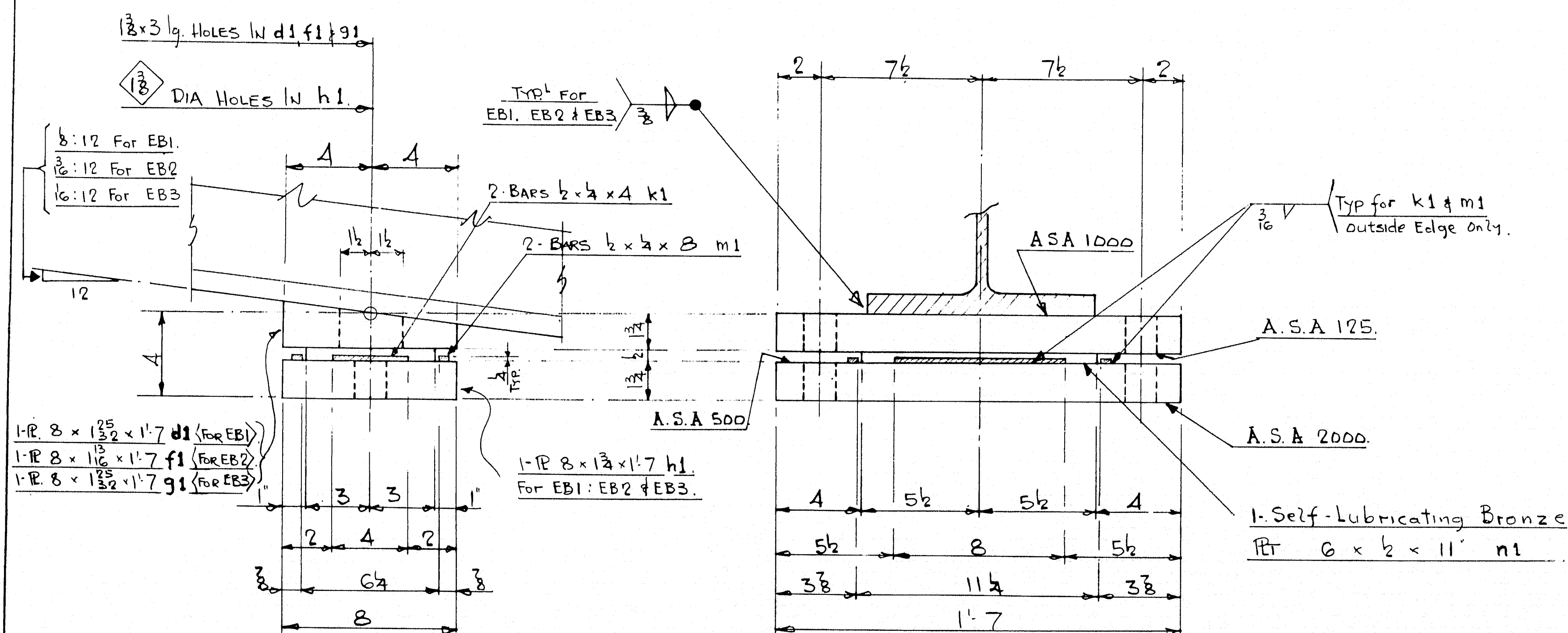
86-200A



72 - ANCHOR BOLTS THUS WITH RT WASHER
24 - ANCHOR BOLTS THUS WITH RD WASHER

6 - FIXED BEARINGS - FB1 {Location: Lines a:b:c:d:e:f Abut #1 (North Bound)}

6 - FIXED BEARINGS - FB2 {Location: Lines a:b:c:d:e:f Abut #1 (South Bound)}



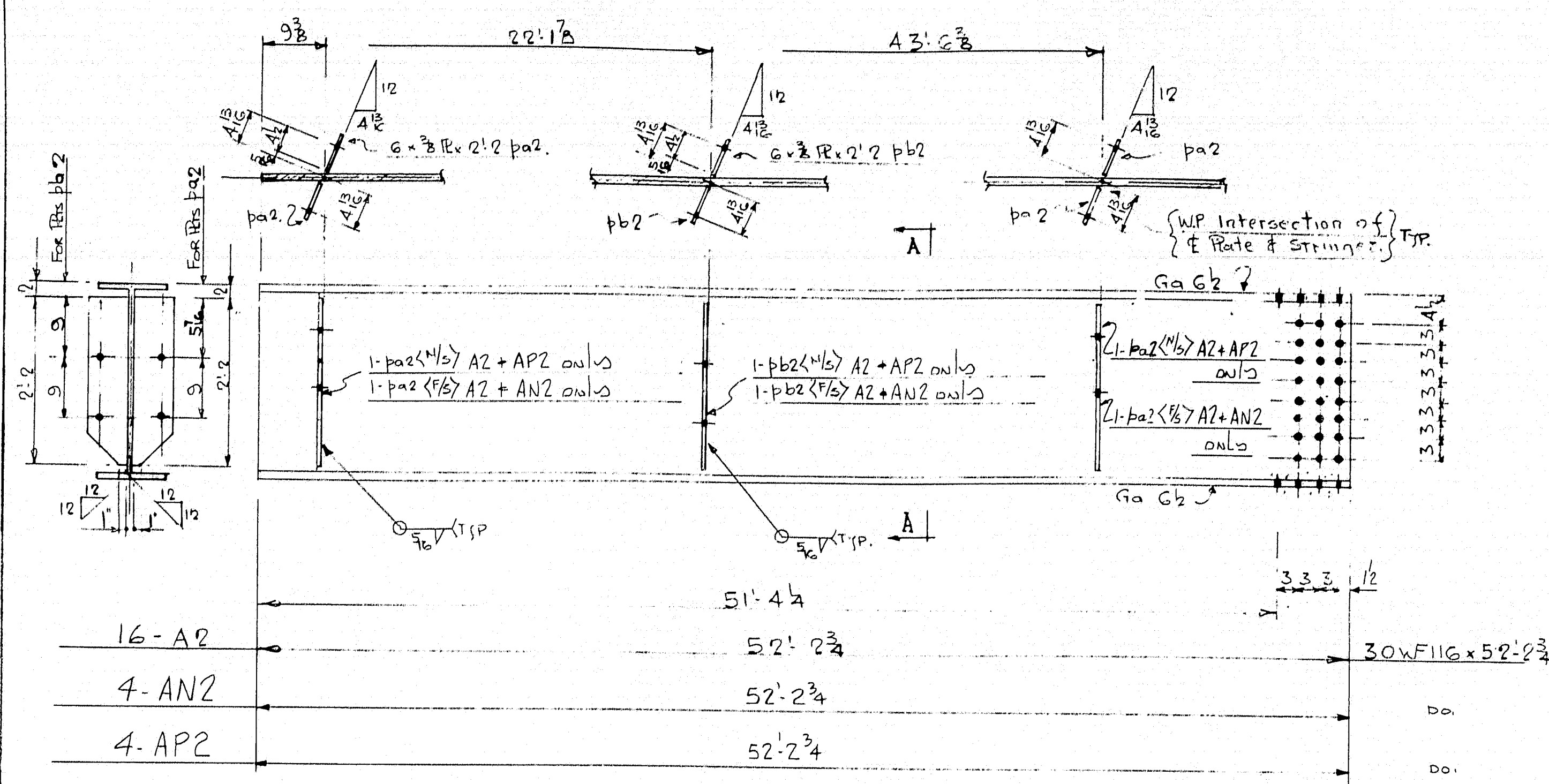
18 - EXPANSION BEARINGS EB1 {Location: Lines a:b:c:d:e:f Abut #2 North Bound
Lines a:b:c:d:e:f Pier #2 North Bound
Lines a:b:c:d:e:f Pier #1 South Bound}

6 - EXPANSION BEARINGS EB2 {Location: Lines a:b:c:d:e:f Pier #1 North Bound}

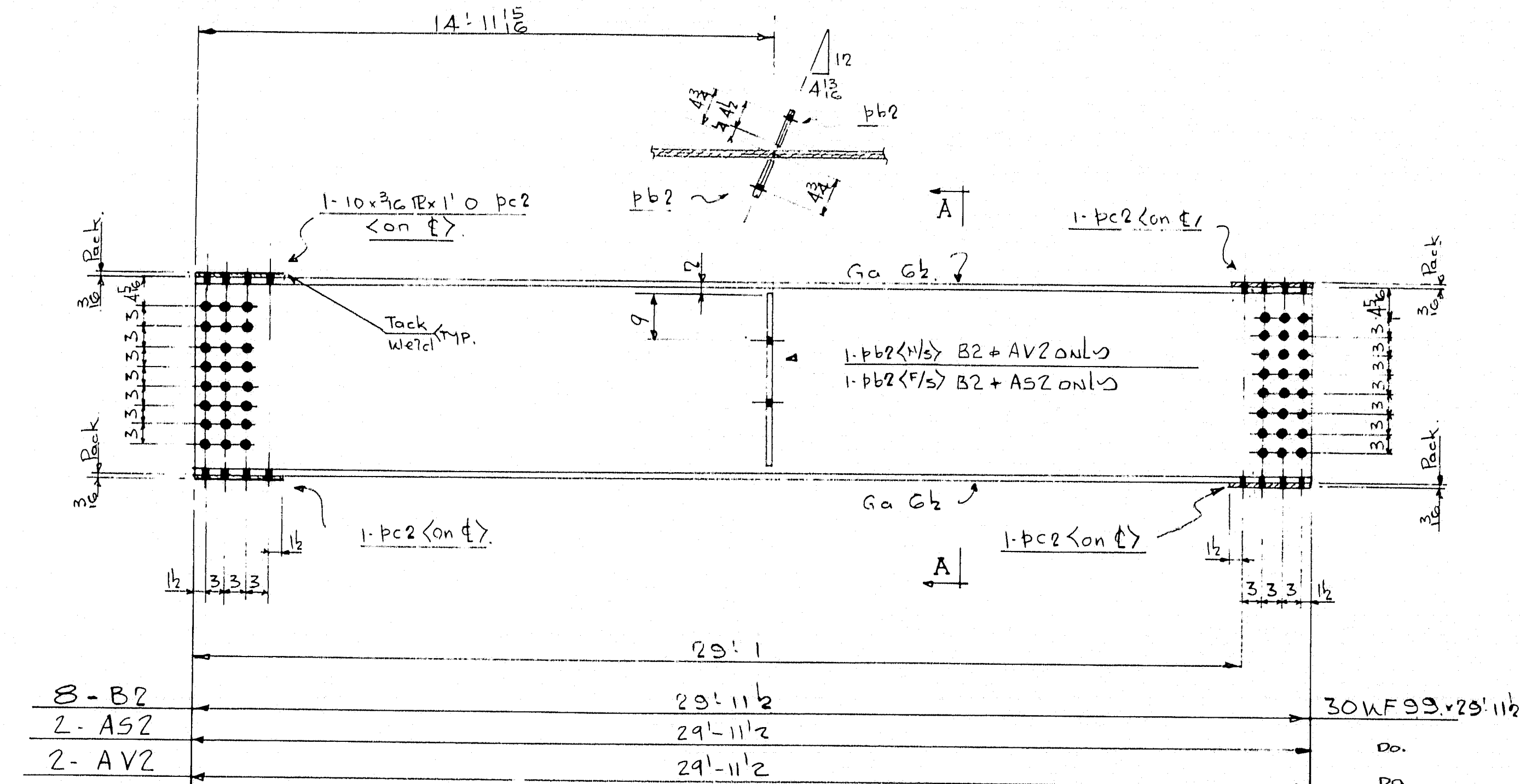
12 - EXPANSION BEARINGS EB3 {Location: Lines a:b:c:d:e:f Pier #2 South Bound
Lines a:b:c:d:e:f Abut #2 South Bound}

REVISIONS	DESCRIPTION	BEARING DETAILS
A	JOB:	BRIDGE OVER NORTH MAIN STREET
B		PITTSFIELD MAINE
C	ARCHT. STATE OF MAINE BRIDGE DEPT.	
E	CUSTOMER:	REED & REED
F		BATH - MAINE
G	AUGUSTA IRON WORKS	ORDER 968
H	AUGUSTA, MAINE	
J		

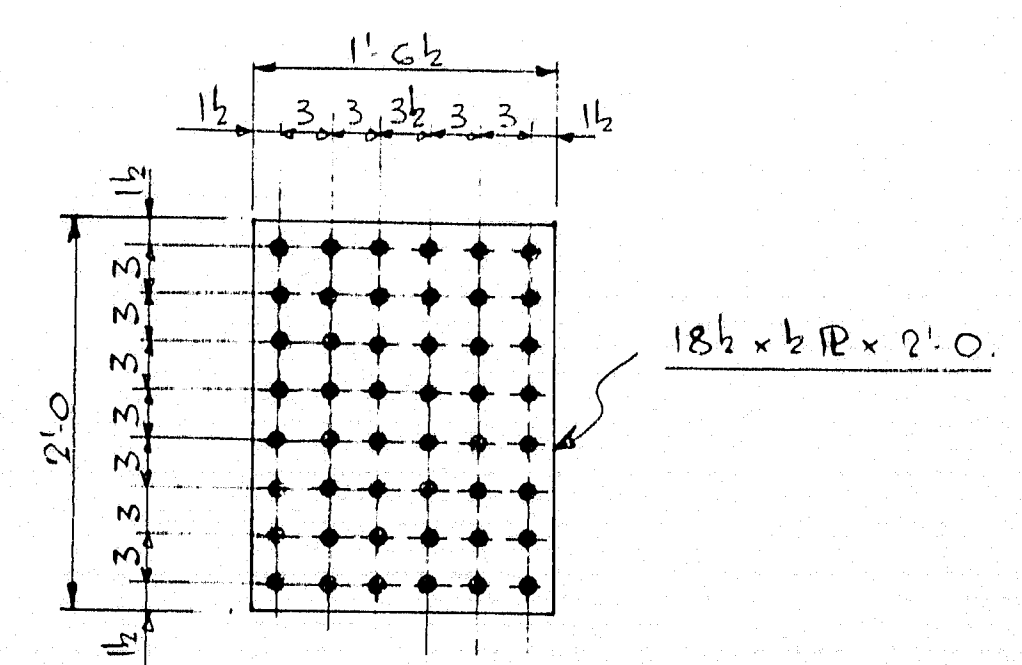
86-200B



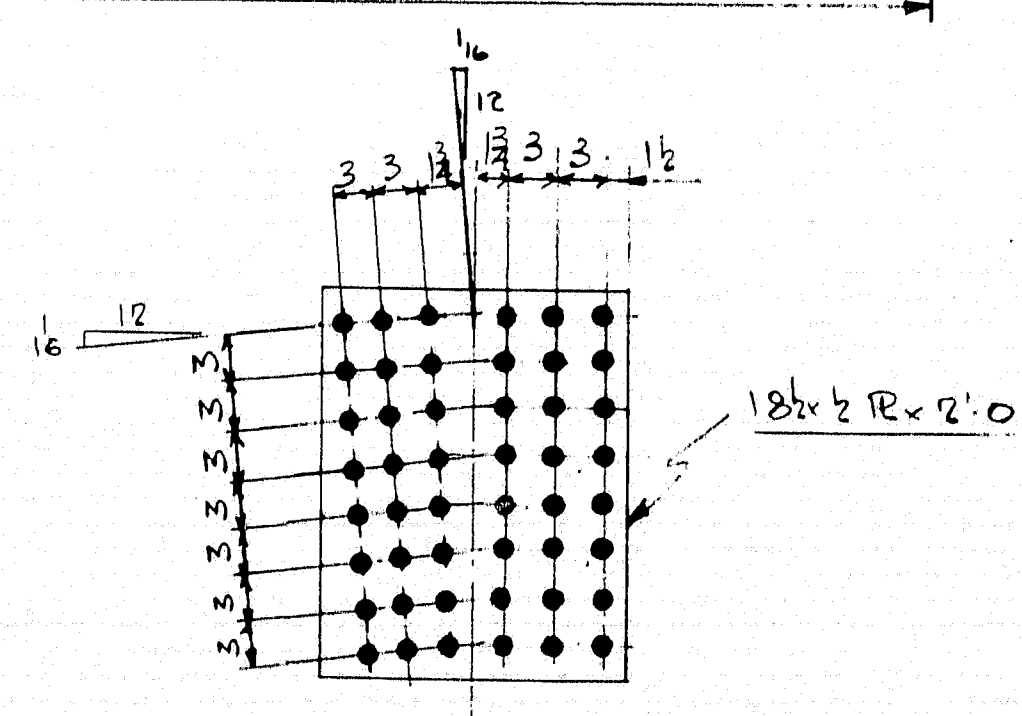
16-A2	51'-4 1/4"	30x116x52'-2 3/4"
4-AN2	52'-2 3/4"	DO
4-AP2	52'-2 3/4"	DO



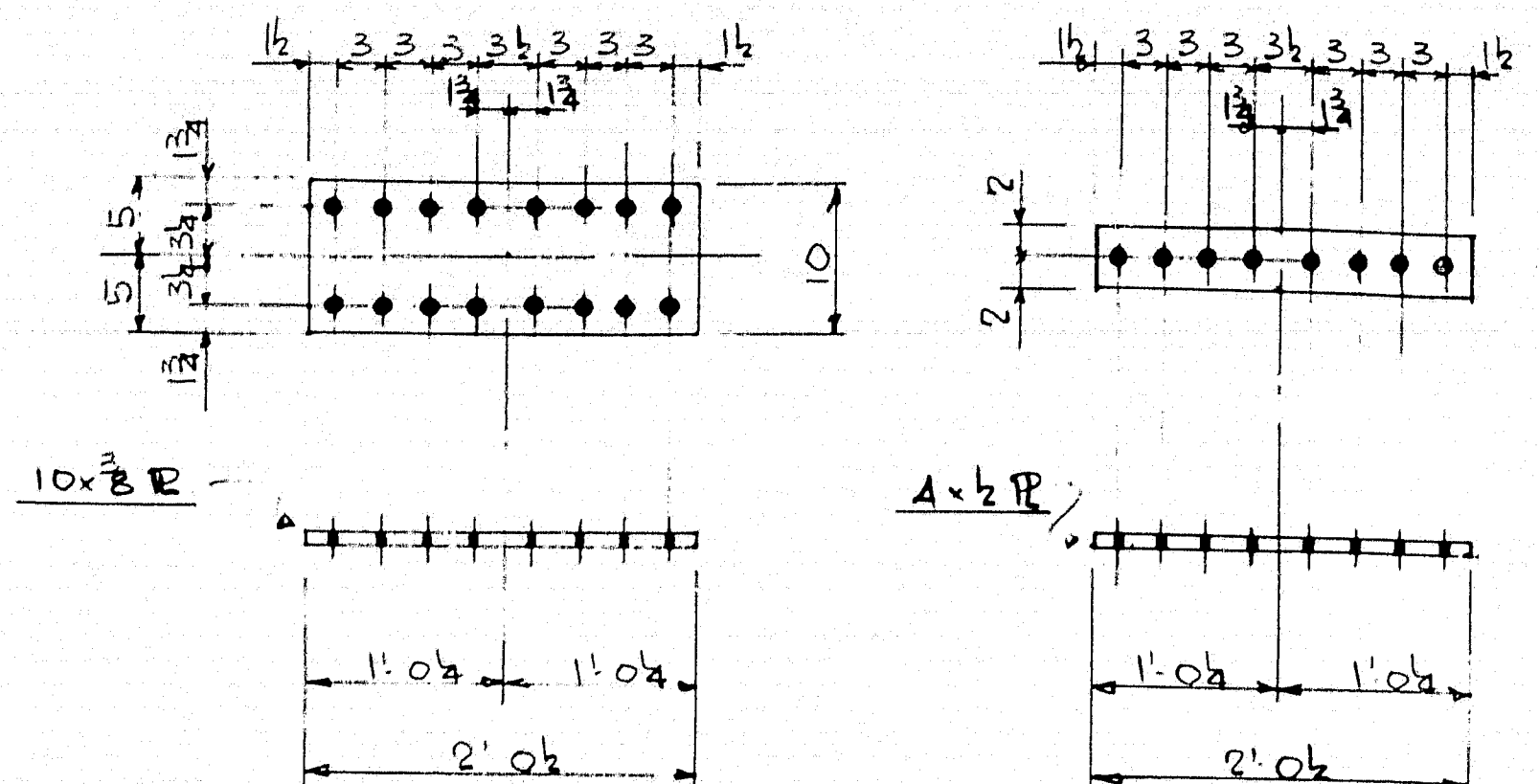
8-B2	29'-11 1/2"	30x116x29'-11 1/2"
2-AS2	29'-11 1/2"	DO
2-AV2	29'-11 1/2"	DO



24-SPLICE PLS THUS 1X P C2.



24-SPLICE PLS THUS 1X P D2.

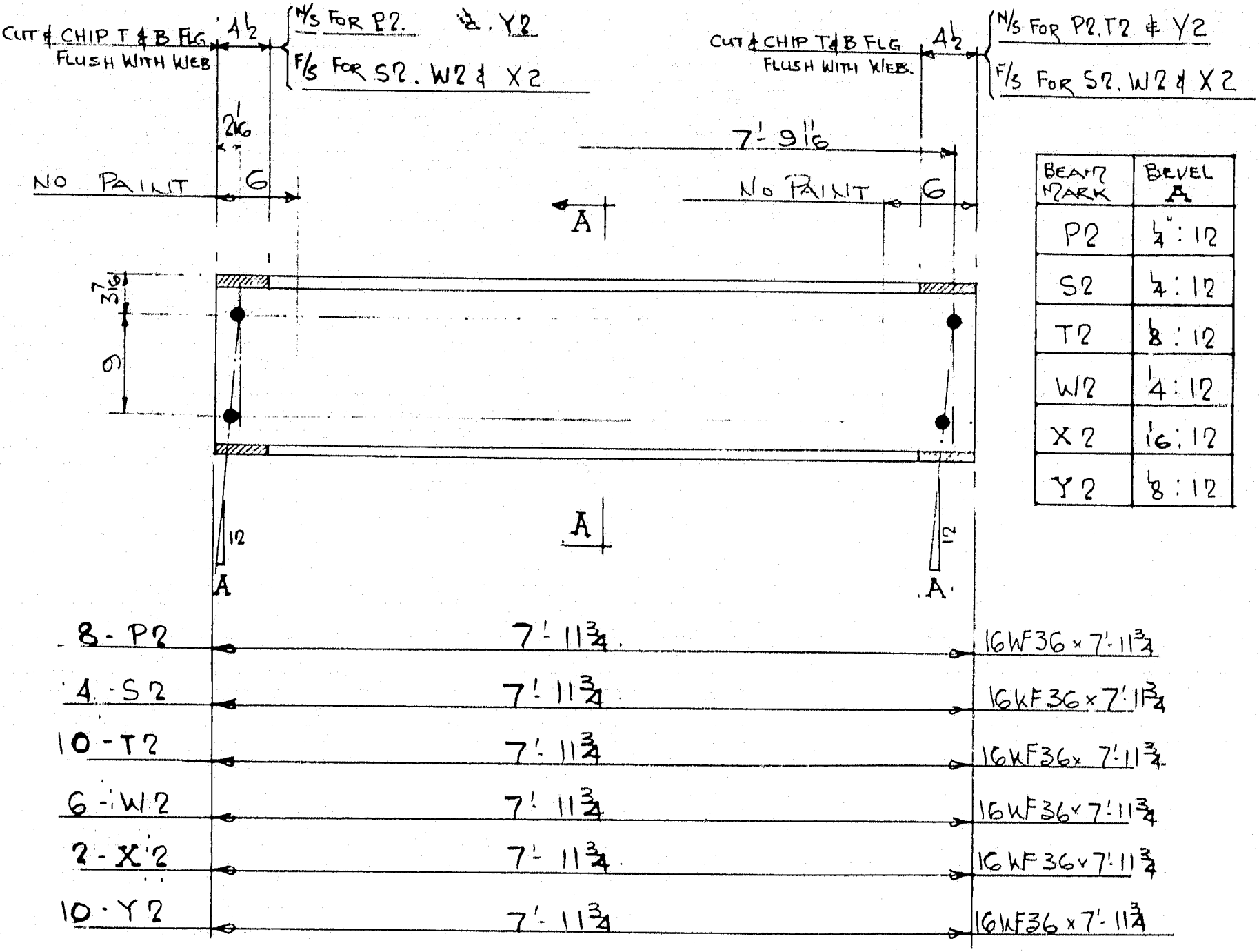


24-SPLICE PLS THUS 1X P E2

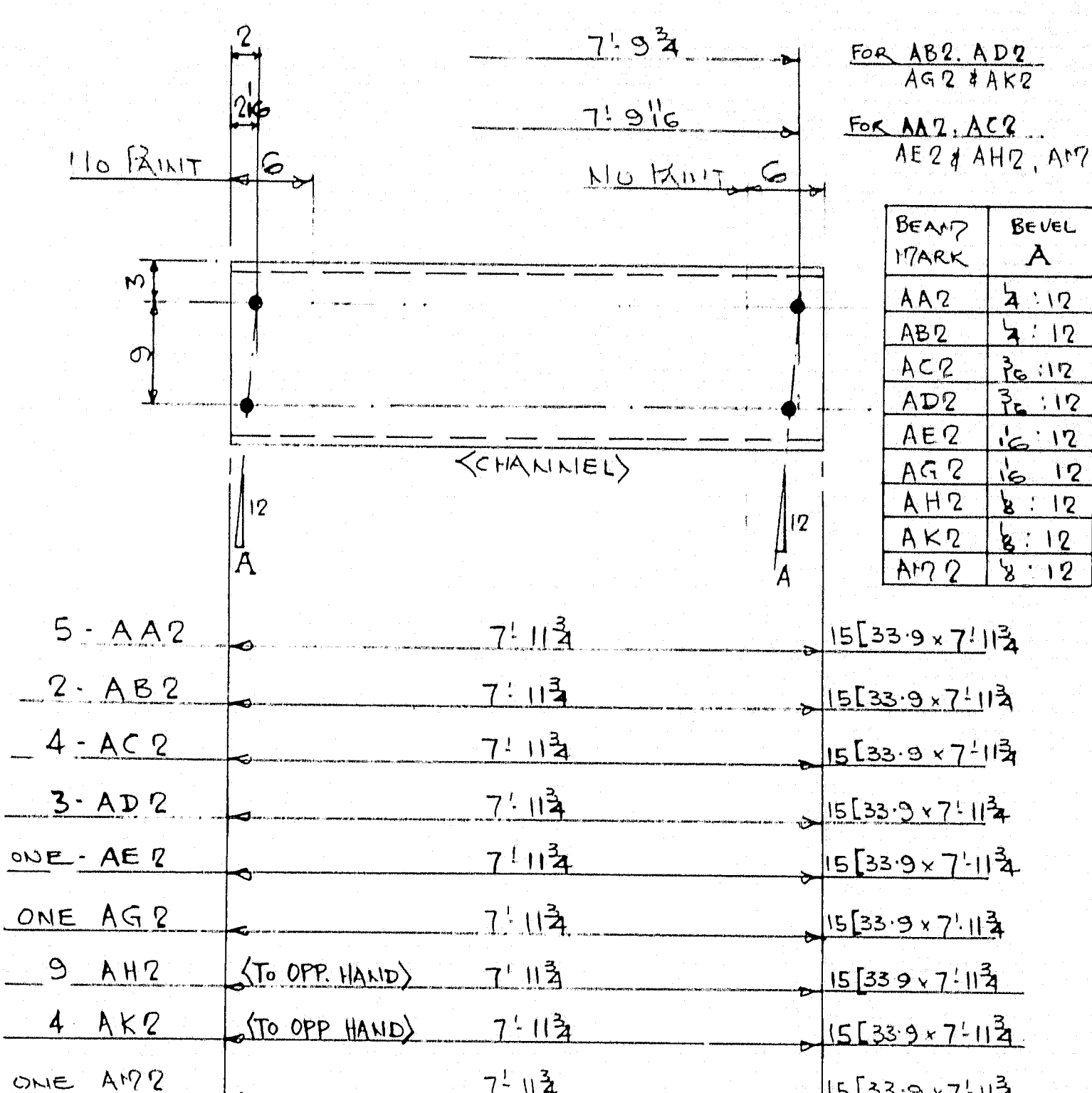
48-SPLICE PLS THUS 1X P G2

12-SPLICE PLS THUS 1X P H2
12-SPLICE PLS THUS 1X P K2

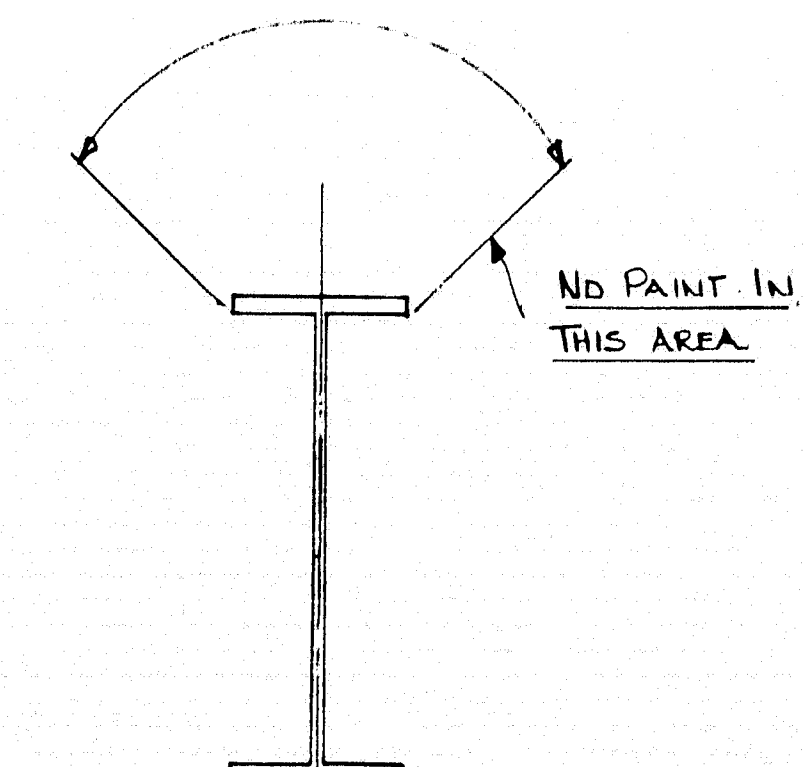
24-SPLICE PLS THUS 1X P I2
24-SPLICE PLS THUS 1X P J2



BEAM MARK	BEVEL A
P2	4:12
S2	4:12
T2	8:12
W2	4:12
X2	6:12
Y2	8:12



BEAM MARK	BEVEL A
AA2	4:12
AB2	4:12
AC2	8:12
AD2	8:12
AE2	4:12
AG2	4:12
AH2	8:12
AK2	8:12
AM2	8:12



SECTION A-A

PAINT
STD RED PRIMER
EXCEPT WHERE NOTED

ALL HOLES 1 1/2" DIA.
BOLTS IN STRINGER SPICE 3/4" HS
BOLTS IN DIAPHRAGMS 3/4" COM 1/2"

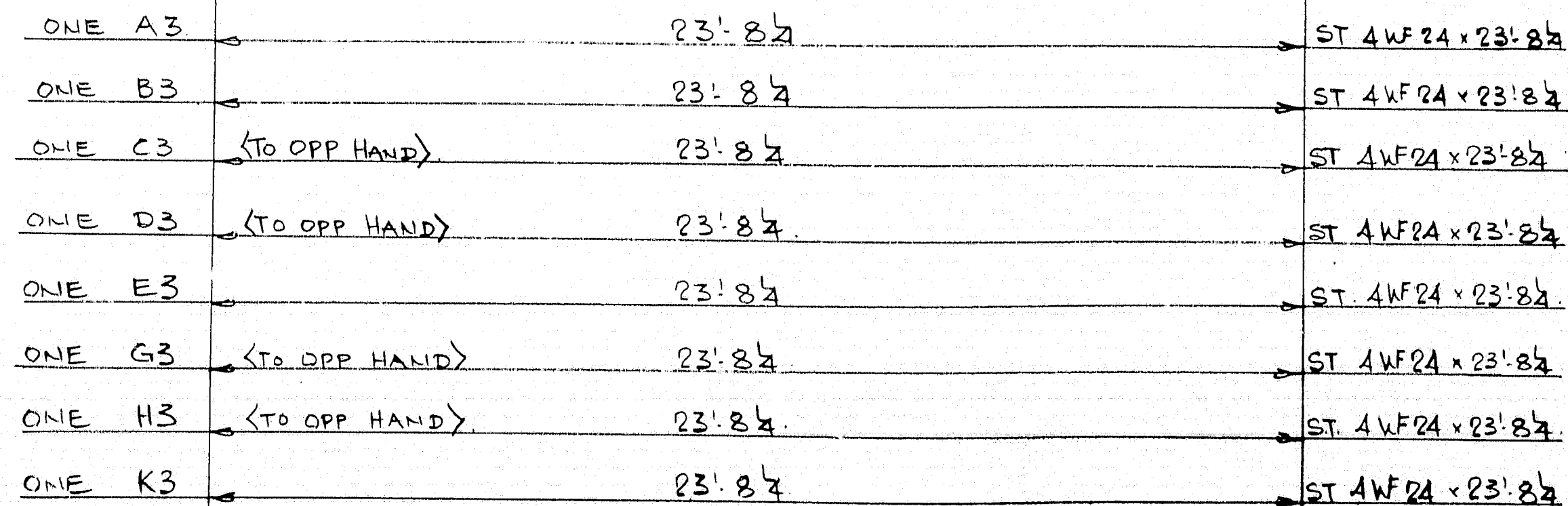
REVISIONS	DESCRIPTION	STRINGER + SPLICE PLT DETAILS
A	JOB	BRIDGE OVER NORTH MAIN STREET
B	DESIGN	PITTSFIELD MAINE
C	CHECKED BY	JK
D	DATE	
E	ARCHT.	STATE OF MAINE BRIDGE DEPT.
F	CUSTOMER	REED + REED
G		BATH - MAINE
H		AUGUSTA IRON WORKS
I		AUGUSTA, MAINE
J		ORDER 968

86-200C

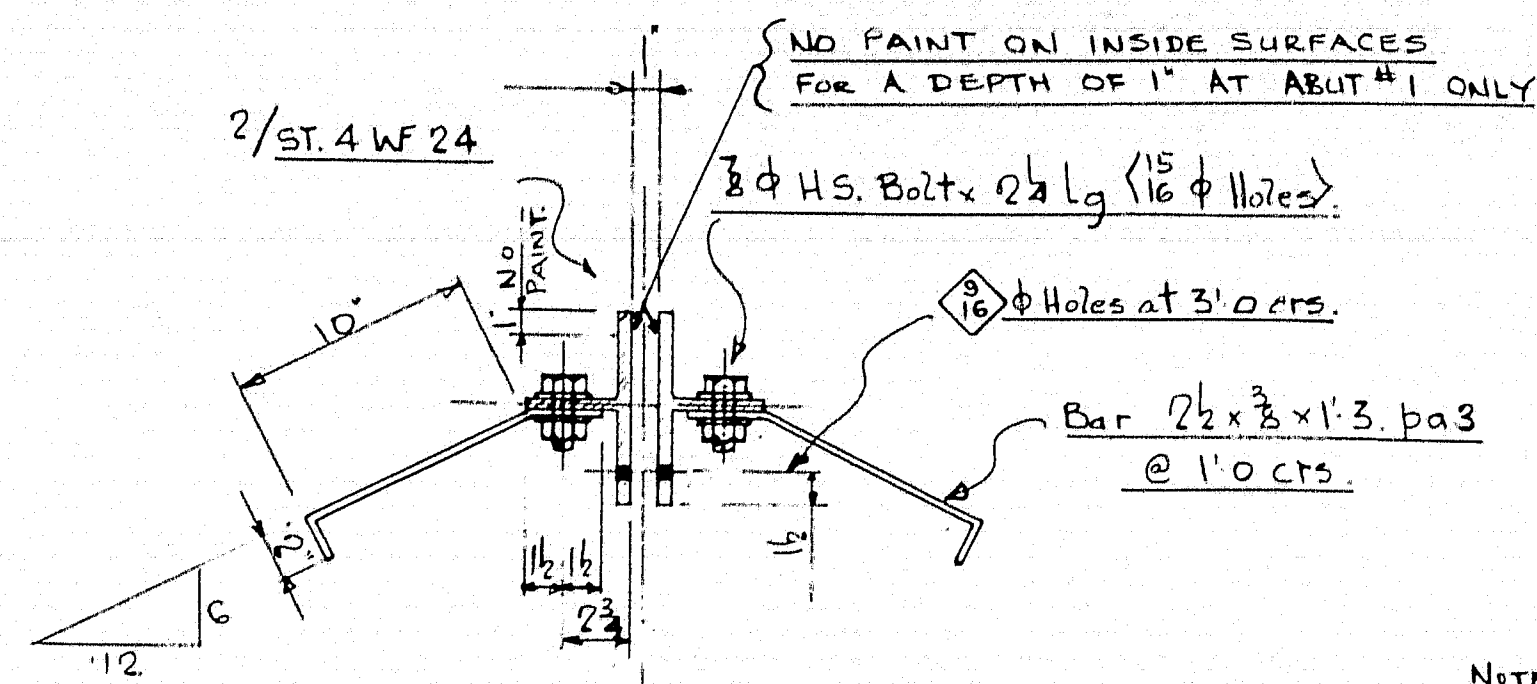
Hand-drawn plan view of a bridge structure. The bridge consists of two abutments and two piers. The structure is oriented diagonally across the page.

- Abutment #1 (Top Left):** Labeled "Bearing Abut #1". It features a right-angled triangle with a vertical side of 12 and a hypotenuse of 13. The angle between the horizontal and the hypotenuse is $21^{\circ} 48'$. A dashed line labeled "fount" is shown near the abutment.
- Abutment #2 (Top Right):** Labeled "Bearing Abut #2". It features a right-angled triangle with a vertical side of 12 and a hypotenuse of 13. A dashed line labeled "fount" is shown near the abutment.
- Pier 1 (Bottom Left):** A small triangle with a vertical side of 12 and a hypotenuse of 13. A dashed line labeled "fount" is shown near the pier.
- Pier 2 (Bottom Right):** A small triangle with a vertical side of 12 and a hypotenuse of 13. A dashed line labeled "fount" is shown near the pier.
- Construction South Bound:** A dashed line labeled "Construction South Bound" runs horizontally across the middle of the diagram.
- Construction North Bound:** A dashed line labeled "Construction North Bound" runs horizontally across the bottom of the diagram.
- SEE DETAIL A:** A label with an arrow pointing to the intersection of the two horizontal construction lines.
- Surveying Data:** Various bearings and angles are noted along the bridge structure. For example, at the top left, the bearing is $N 43^{\circ} 10' E$ and the angle is $43^{\circ} 10'$. At the bottom left, the bearing is $N 43^{\circ} 10' E$ and the angle is $43^{\circ} 10'$. At the bottom right, the bearing is $N 43^{\circ} 10' E$ and the angle is $43^{\circ} 10'$.

LAYOUT OF ARMORED JOINTS.

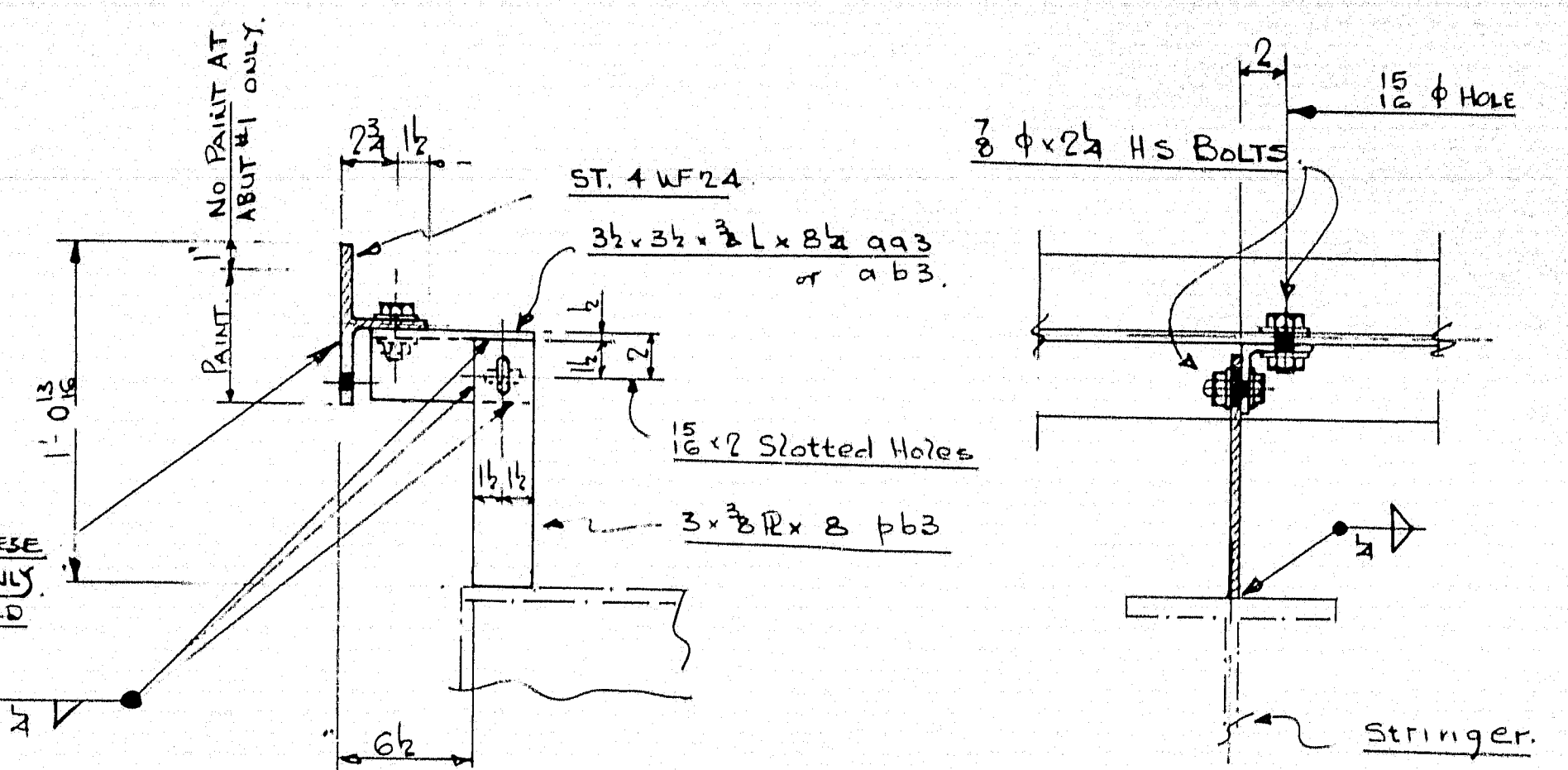


<u>SHOP NOTE</u>				
K3 Is The Same as A3 Except For Painting				
E3	5	5	B3	5
G3	5	5	C3	5
H3	5	5	D3	5

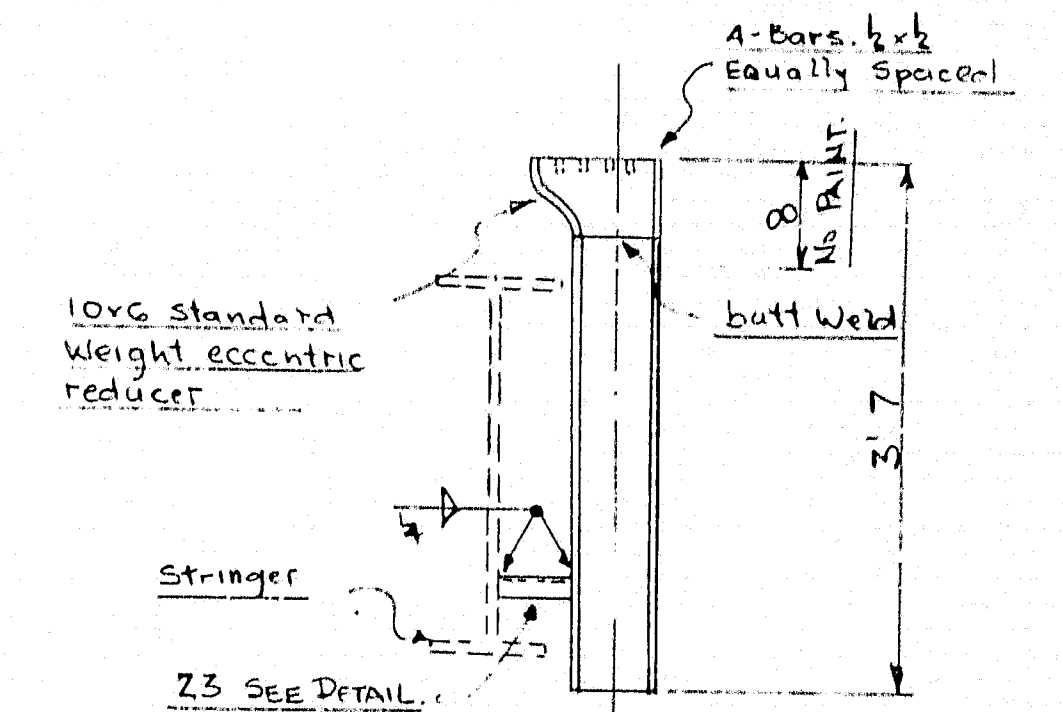


Hand-drawn technical drawing of a bridge deck cross-section showing the layout of three sets of brackets (aa3, bb3, cc3) for shop bolts. The drawing includes dimensions for bracket spacing (18' 3"), bolt spacing (10' 8" and 19' 1 1/2"), and deck width (34' 0"). It also shows the placement of shop bolts (aa3, bb3, cc3) and the location of 3/8" holes at 3'0" centers. Annotations include "PAINT THIS SURFACE ONLY", "NO PAINT", and "CUT & CHIP".

ONE L3	19' 4 1/2	ST 4WF24 x 19' 4 1/2
ONE P3	19' 4 1/2	ST 4WF24 x 19' 4 1/2
ONE N3	(TO OPP HAND)	ST 4WF24 x 19' 4 1/2
ONE P3	(TO OPP HAND)	ST 4WF24 x 19' 4 1/2
ONE S3	19' 4 1/2	ST 4WF24 x 19' 4 1/2
ONE T3	19' 4 1/2	ST 4WF24 x 19' 4 1/2
ONE W3	(TO OPP HAND)	ST 4WF24 x 19' 4 1/2
ONE X3	(TO OPP HAND)	ST 4WF24 x 19' 4 1/2

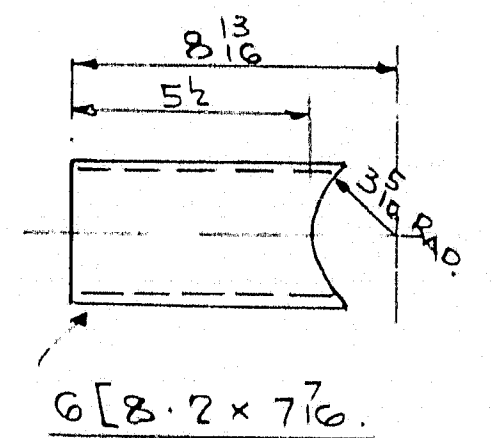


DETAIL OF JOINT ADJUSTMENT.



6" DIA. Std. Pipe x 3'7" ALL.

12 - DRAINS THUS MK Y3.



12. CHANNELS THIS MK Z3.

PAINT STD. RED PRIMER EXCEPT
WHERE NOTED
1/2" ϕ HOLES FOR 3/4" ϕ H.S. BOLTS UNLESS NOTED

REVIEWS	DESCRIPTION: ARMORED JOINT DETAILS		DRAWN BY	DATE
A	JOB:		J.M.	
B	BRIDGE OVER NORTH MAIN STREET		CHECKED BY	DATE
C	PITTSFIELD MAINE		J.K.	
D	ARCHT. STATE OF MAINE BRIDGE DEPT.			
E	CUSTOMER: REED & REED		SHEET	3
F	BATH - MAINE			
G	AUGUSTA IRON WORKS		ORDER	968
H	AUGUSTA, MAINE			
J				